

Tighe & Bond

DRAFT

8 and 20 Canal Road
Montague, Massachusetts

**Strathmore Mill Complex
Demolition
Project Notification Form (PNF)**

**Town of Montague; Village of Turners
Falls**

June 2026

M-5003-0012-01.01D
June 18, 2026

State Historic Preservation Officer
Massachusetts Historical Commission
The Massachusetts Archive Building
220 Morrissey Boulevard
Boston, MA 02125

**Re: Strathmore Mill Complex Demolition Project
Project Notification Form (Revised)
MHC# RC.66308
8 and 20 Canal Road, Village of Turners Falls, Montague, Massachusetts**

To Whom It May Concern:

On behalf of the Town of Montague (Town), Tighe & Bond respectfully submits this Revised Project Notification Form (PNF) for the demolition of the Strathmore Mill Complex (Mill Complex) in the Village of Turners Falls, Town of Montague, Franklin County, Massachusetts. A previous PNF was submitted and reviewed in April 2019. Since that time, the scope of work has increased due to further structural deterioration of buildings and additional funding for demolition and hazardous materials abatement. The project will be undertaken with state and federal funding and the Town will apply to local and state agencies for project permits and is submitting this PNF to the Massachusetts Historical Commission (MHC; the State Historic Preservation Officer) as the Mill Complex includes contributing structures to a National Register Listed Historic District. The Town is seeking MHC's concurrence that the Mill Complex is so extensively damaged and/or degraded that the proposed abatement and demolition activities are warranted and necessary to protect public health and safety (including that of first responders to a potential emergency) and to avoid additional uncontrolled structure collapse within an environmentally sensitive area.

This submittal describes activities for the demolition of eleven (11) buildings and ancillary structures within the Strathmore Mill Complex. Structures and areas scheduled for demolition include Buildings #1, #2, #3, #4, #5/5A, #6/6A, #7, #8, and #11, the base of the Smokestack in Courtyard (East), and the Loading Dock attached to Building #3. A separate "off-site" building (referred to as Building #9) is connected to the Mill Complex on a separate privately-owned parcel and contains an active water power wheel and is scheduled to remain. Building locations are identified on Figure 2 in Attachment B.

During the preliminary design and planning process for the demolition project, the Town coordinated with the Montague Historic Commission to identify feasible mitigation measures for the demolition of the buildings at the Project Site. In November 2025, the Town and the Chair of the Montague Historical Commission identified the following mitigation items for incorporation into the demolition design:

- Strategic preservation of building foundations and footprints to the extent practical: The demolition design is working to incorporate preservation of building foundations and footprints to the extent practicable. Riverside building foundations for Buildings #1, #2 and #3 will remain in place.
- Provision of a historically appropriate facade on the newly exposed wall(s) of Building #9: The project includes a brick facade for the newly exposed walls of Building 9, with the possibility of incorporating existing bricks from the Project Site into the design of the facade.

- Photographic preservation of the interiors of the Strathmore Structures: A photographic log that documents the interior design of the structures prior to demolition will be prepared.

A copy of the Project Notification Form (PNF) is provided in Attachment A, figures showing the site location and geographic context of the Project Site in Attachment B, and site photographs of existing conditions within the Project Area in Attachment C. Attachment D contains relevant pages, record documents, inventory forms, and National Register of Historic Places nominations from the Massachusetts Cultural Resource Inventory System (MACRIS) as they pertain to the historic districts and properties at the Project Site. Attachment E contains the Massachusetts Historical Commission's response letter to the PNF submitted in April 2019 for an earlier version of this project as well as additional correspondence regarding the removal of the Smokestack in 2020.

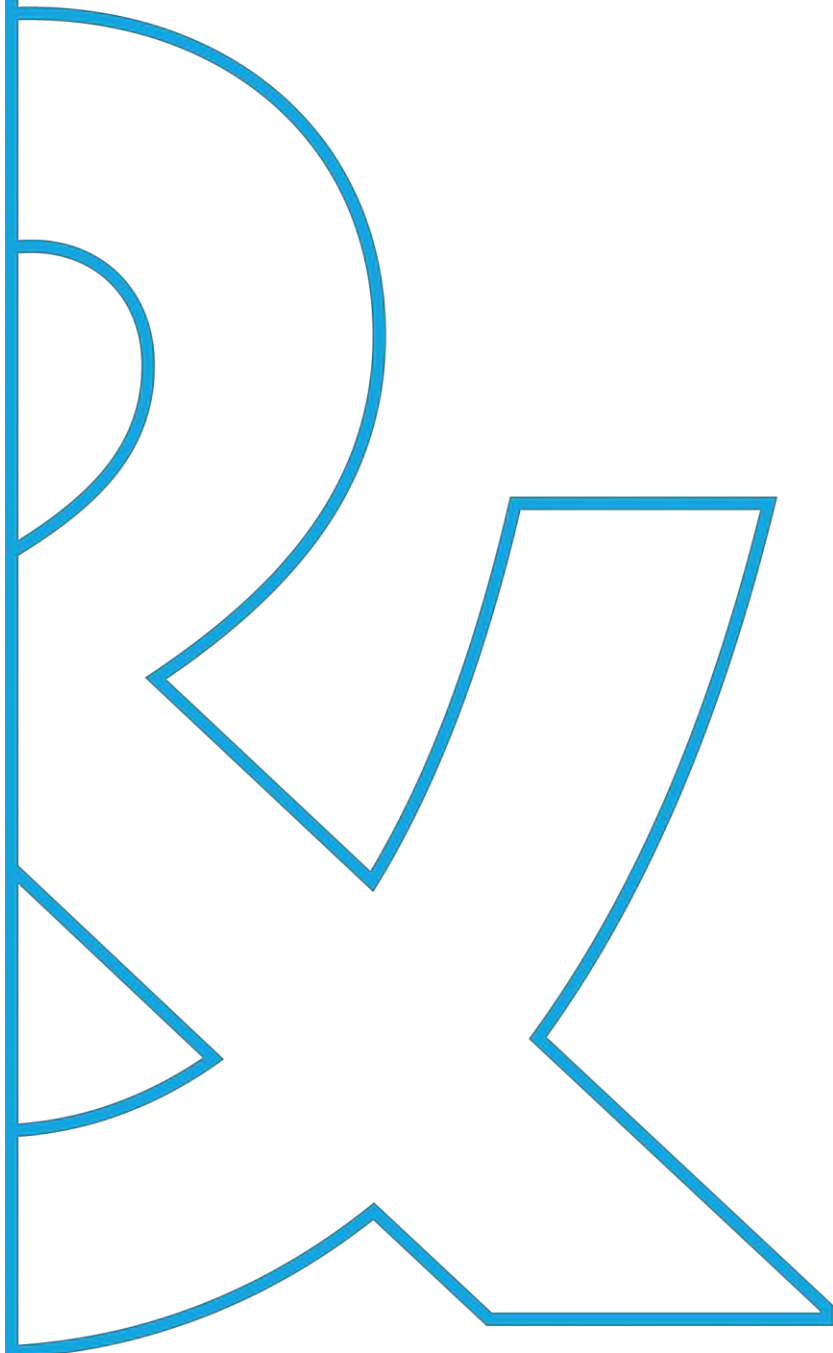
Thank you for your attention to this request. Should you have any questions or require additional information for your review please contact me at (413) 572-3256 or by email at tjadamski@tighebond.com.

Very truly yours,

Tracy J. Adamksi
Vice President

Enclosures

Copy: Bettina Washington, THPO Wampanoag Tribe of Gay Head (Aquinnah)
David Weeden, THPO Mashpee Wampanoag Tribe
Mark Andrews, Deputy THPO Narraganset Indian Tribe
Jeff Bendremer, THPO Historic Preservation Manager, Stockbridge-Munsee Community
Walter Ramsey, Town of Montague
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Attachments

Attachment A: Form

Attachment B: Figures and Project Drawings

- Figure 1: Site Locus (February 2026)
- Figure 2: Orthophotograph (April 2026)
- Figure 3: Historical Resources (March 2026)
- Project Drawings

Attachment C: Site Photographs

Attachment D: Sanborn Fire Insurance Maps

- Sheet 1: Turner’s Falls, MA (1884)
- Sheet 2: Turner’s Falls, MA (1889)
- Sheet 3: Turner’s Falls, MA (1902)
- Sheet 4: Turner’s Falls, MA (1914)
- Sheet 5: Turner’s Falls, MA (1940)

Attachment E: MHC Correspondence

- Determination of Adverse Effect (April 2019)
- Letter of Clarification regarding MHC’s Determination (June 2020)



Section 1

SECTION 1 | Introduction

The Project Site (42.61043, -72.55820) is located in the Village of Turners Falls at 8 and 20 Canal Road. The majority of the Mill Complex is located at 20 Canal Road. The proposed project will occur within the limits of two lots (Parcels: 02-0-01 and 02-0-06) owned by the Town; these parcels comprise the project locus. For reference, a USGS Site Location Map (Figure 1), an Orthophotograph Plan (Figure 2), and Historical Resources (Figure 3) are provided in Attachment B.

1.1 Project Background and Purpose

The proposed project entails the demolition of eleven (11) buildings and ancillary structures associated with the Mill Complex. Buildings #1, #2, #3, #4, #5/5A, #6/6A, #7, #8, and #11, the base of the Smokestack located in Courtyard (East), and the Loading Dock attached to Building #3 are scheduled for demolition. The abandoned Tailrace #3 beneath Building #3 is also scheduled to be decommissioned (capped in place).

The Town has received state and federal funding for building demolition and is in the design and permitting stage for the demolition of the Mill Complex in preparation for future redevelopment of the site. The majority of the Mill Complex has been vacant since 2007 when an arson fire damaged multiple buildings, including the complete destruction of former Building #10. The Town took ownership of the Mill Complex in 2010 as a result of a lapse in tax payments by the owner at that time. Vacant and unused, the Mill Complex is vulnerable to trespassers and vandalism, an issue the Town has spent thousands of dollars combatting. In the “Strathmore Mill Feasibility Study For the Town of Montague, Massachusetts”, dated May 2005 prepared by Finegold Alexander + Associates, Inc., FXM Associates, Ajax Investment Partners, LLC, Tighe & Bond, Inc., and Allied Consulting Engineering Services, Inc, and the “Site Development Assessment Report”, dated August 12, 2008 and prepared by Fuss & O’Neill, structural deficiencies in each building within the Mill Complex were identified. Descriptions of each building scheduled for demolition, including a summary of deficiencies noted in 2005 and 2008, are provided in Section 2. The structural integrity of the site buildings and structures have continued to decline in the intervening years.

The deteriorating condition and underutilization of the Mill Complex space has driven the Town to consider potential redevelopment of the land. Due to the compromised structural integrity of the buildings, the Town is proposing to demolish the majority of the structures to limit the potential for harm to public health, safety, and the environment that could occur in the event of an uncontrolled building collapse. Furthermore, requirements to rehabilitate the buildings to meet current codes, and limited site access and parking, significantly restrict the redevelopment and reuse potential of the buildings. Therefore, the Town is proposing the demolition of the majority of the structures at the site.

In Spring 2019, Tighe & Bond submitted a Project Notification Form (PNF) to the Massachusetts Historical Commission (MHC) of for the demolition of Strathmore Mill Complex buildings #3, #5/5A, #6/6A, #7, #8, the Smokestack in Courtyard (East), and the loading dock at building #3. MHC determined that the demolition of these structures constituted an “adverse effect” and initiated the MHC’s consultation process pursuant to 950 CMR 71.07(3). Additionally, the Smokestack in Courtyard (East) was removed in 2020 using municipal funds due to concerns for public health and safety related to the Smokestack’s structural integrity issues. The base of the Smokestack remains in Courtyard (East) and is also proposed to be demolished. Copies of the MHC PNF Response Letters and correspondence about the removal of the Smokestack are provided in Attachment E. Due to funding constraints, further demolition planning and design was postponed until now.

1.1.1 Change in Proposed Scope of Demolition

The proposed project has since been expanded to include the demolition of Buildings #1, #2, #4 and #11 along with the previously-proposed buildings due to their continued deterioration and the limited feasibility of potential reuse options for these structures. As the connected Building #9 is under private ownership with an active power water wheel, it will remain. An impact analysis conducted by Tighe & Bond indicated that the demolition of all buildings surrounding Building #9 (i.e., Buildings #5/5A, #2 and #4) will result in the need to construct substantial reinforced concrete and concrete masonry unit (CMU) walls to resist the force of exterior loading around Building #9. Additionally, a new stair tower would need to be constructed to supplement required egress out of the building. This option was considered more feasible (and cost effective) than to include the demolition of Building #9 with construction of a new building to house the hydro turbine and the required support equipment for that off-site building.

To provide the Town with a feasible solution to demolish numerous buildings within the Project Site using the available Project funding (with the possibility that additional funding may become available as the project advances to construction), a tiered approach of building demolition was established that progressively increases the number of buildings to be demolished. Table 3-1 summarizes the demolition alternates in the progressive order of occurrence for bidding.

TABLE 1-1 Proposed Demolition Alternates

Demolition Alternates	Scheduled to be Demolished	Scheduled to Remain	New Construction Needed	Other Repair Work Needed
Base Bid	Building #s: 3, 5/5a, 6/6a, 7, and 8; Base of Smokestack in Courtyard (East); Loading Dock attached to Building #3	Buildings #s: 1, 2, 4, and 11; Tailrace No. 2 system beneath Building #2; Concrete Walled Tank west of Building #1.	Reinforced concrete retaining wall below street level grade, and new CMU wall with brick veneer along eastern side of Building #9; For Tailrace No. 3 beneath Building #3, seal with CMU wall at property line and fill in with flowable concrete fill.	Fill in doorway openings, etc. along existing walls at the newly exposed southern and eastern sides of Building #2; Fill in subsurface clearwater holding tank in Courtyard East with flowable concrete fill.
Bid Alternate No. 1	Building #s: 1, 3, 5/5a, 6/6a, 7, and 8; Concrete Walled Tank west of Building #1; Base of Smokestack in Courtyard (East); Loading Dock attached to Building #3.	Building #s: 2, 4, and 11;	Reinforced concrete retaining walls below street level grade, and new CMU walls with brick veneer along eastern side of Building #9; A new stair tower system at the west side of Building #2 to supplement the required egress out of Building #9 connection; For Tailrace No.3 beneath Building #3, seal with CMU wall at property line and fill in with flowable concrete fill.	Fill in doorway openings, etc. along existing walls at the newly exposed southern, eastern and western sides of Building #2; Fill in subsurface clearwater holding tank in Courtyard East with flowable concrete fill.

Demolition Alternates	Scheduled to be Demolished	Scheduled to Remain	New Construction Needed	Other Repair Work Needed
Bid Alternate No. 2	<p>Building #s: 1, 3, 5/5a, 6/6a, 7, 8, and 11;</p> <p>Concrete Walled Tank west of Building #1;</p> <p>Remaining pedestal base of Smokestack in East Courtyard;</p> <p>Loading Dock attached to Building #3.</p>	<p>Building #s: 2 and 4;</p>	<p>A new stair tower system at the west side of Building #2 to supplement the required egress out of Building #9 connection;</p> <p>Reinforced concrete retaining walls below street level grade, and new CMU walls with brick veneer along eastern side of Building #9;</p> <p>For Tailrace No.3 beneath Building #3, seal with CMU wall at property line and fill in with flowable concrete fill.</p>	<p>Fill in doorway openings, etc. along existing walls at the newly exposed southern, eastern and western sides of Building #2;</p> <p>Fill in subsurface clearwater holding tank in Courtyard East with flowable concrete fill.</p>
Bid Alternate No. 3	<p>Building #s: 1, 2, 3, 4, 5/5a, 6/6a, 7, 8, and 11;</p> <p>Concrete Walled Tank west of Building #1;</p> <p>Remaining pedestal base of Smokestack in East Courtyard;</p> <p>Loading Dock attached to Building #3.</p>	<p>Tailrace No. 2 system beneath Building #2.</p>	<p>Reinforced concrete retaining walls below street level grade, and new CMU walls with brick veneer along eastern, northern, and western sides of Building #9;</p> <p>A new stair tower system at the rear (north side) of Building #9 to supplement the required egress out of the building;</p> <p>For Tailrace No.3 beneath Building #3, seal with CMU wall at property line and fill in with flowable concrete fill.</p>	<p>Fill in subsurface clearwater holding tank in Courtyard East with flowable concrete fill.</p>

HISTORIC PRESERVATION STRATEGY

In November 2025, the Town met with the Montague Historical Commission to discuss the project. Historic preservation strategies were discussed and three items were agreed upon:

- Strategically preserve building foundations and footprints to the extent practical
- Provide a historically appropriate facade on the newly exposed wall(s) of Building #9, possible re-use of onsite bricks
- Interest in interior photographic preservation of the Strathmore Structures

The project team has taken these preservation points and incorporated them in the project design where feasible. The demolition design will aim to preserve building foundations, footprints, and retaining walls to the maximum extent practicable. The building foundations for Buildings #1, #2 and #3 along the Connecticut River will remain in place. Additionally, the existing fieldstone foundation walls for Building #s: 4, 5, 6, 7, 8 and 11 along Canal Road will remain. The existing fieldstone foundations from former Building #10 will also remain in place. A two-foot high brick knee wall along the south sides of Buildings #2 and 3 will also remain.

The proposed project includes the implementation of a brick facade for the newly exposed walls of Building #9 that will utilize existing bricks from the project site into the accent pieces. Drawings showing the Historical Preservation Plan and the proposed façade are provided within the Permit Set (Drawings C-300 and HIST-S-301 to HIST-S-303) in Attachment B.

Through this PNF, the Town seeks comments and review from the MHC for the potential demolition alternates at the site outlined in Table 1-1.

1.2 Limitations to Redevelopment

According to “A Technical Assistance Panel Report Strathmore Mill Complex Redevelopment,” dated 2011 and prepared by Urban Land Institute, there are multiple challenges and limitations complicating site redevelopment. Limitations of redevelopment of the Mill Complex include:

- **Limited Site Access and Parking:** The Site access is limited to the 12- to 15-foot wide Canal Road which has weight, height, and width restrictions. Canal Road does not have the capacity to accommodate two-way traffic which makes retail and/or residential re-use infeasible. In addition, the limited access may impede emergency response. There is also limited existing parking capacity and limited availability to create parking areas on site.
- **Financial Feasibility:** Due to the condition of the structures on site, redevelopment costs are estimated to be significantly higher than feasible to maintain an industrial rental property; the market is not capable of supporting these costs without subsidy. In addition, the low population density of Montague was not expected to attract certain uses such as retail or office and the adjacent town, Greenfield, already contained existing retail with similar accessibility to I-91 as Montague.
- **Deterioration of Buildings:** The Mill Complex has not been regularly maintained and would therefore require extensive rehabilitation to meet current building codes. Structural deficiencies are described under the Existing Conditions section above.
- **Outdated Infrastructure:** Existing utilities (e.g., gas, water, sewer, and electrical utilities) would likely require upgrades which could significantly increase redevelopment costs.
- **Legal Issues/Easements:** Current disputes over easement rights of abutting properties may complicate property owner cooperation needed for access and redevelopment.
- **Restricted Location:** The limited land area and abutting waterways limit the scale of potential redevelopment. The Mill Complex is located on a narrow piece of land and is bounded to the north and south by the Connecticut River and Power Canal, respectively.



Section 2

SECTION 2 | Existing Conditions

The Mill Complex is primarily located at 20 Canal Road (Parcel 02-0-01, 1.9 acres) on an island bordered to the north by the Connecticut River, to the south by an active hydroelectric Power Canal, and to the west by buildings occupied by the Southworth Company. A portion of the project is also located at 8 Canal Road (Parcel: 02-0-06; 3.19 acres). The Mill Complex is located within the eastern section of the island which is primarily, and historically, used for industrial purposes.

The Mill Complex consists of nine interconnected buildings that are depicted on various historical plans as Buildings #1 through #9, and another separate building referred to as Building #11, ranging in height from two to seven stories with a total floor area of 245,000 gross square feet. A loading dock connected to the east/northeastern portion of Building #3, that runs immediately adjacent to Building #7 to Canal Road, is located on an adjacent parcel (Parcel 02-0-06) to the east at 8 Canal Road. [Note: Building #9 is privately-owned, is on a separate parcel (Parcel 02-0-01C), and is not scheduled for demolition as part of this project.] Parcel 02-0-06 is bordered to the east by property owned by FirstLight Hydro Generating Company. An Orthophotograph detailing the Mill Complex building numbers and associated areas is provided as Figure 2 in Attachment B. It should be noted that the Connecticut River side of the Mill Complex is referred to as 'Project North'.

A letter to the Town of Montague from the FirstLight Hydro Generating Company dated June 14, 2012 detailed restrictions along the FirstLight Travel Way, a 10-foot wide portion of Canal Road as measured from the vertical waterline, adjacent to the Power Canal. These restrictions are as follows:

No vehicle having maximum wheel loading in excess of six thousand (6,000) pounds with minimum axle spacing of at least four and one-half feet (4.5') shall be permitted passage along the FirstLight Travel Way; and, if the canal is dewatered, (as determined by FirstLight in its sole discretion), no vehicle having maximum wheel loading in excess of four thousand (4,000) pounds shall be permitted passage along the FirstLight Travel Way.

During construction, these weight and axle restrictions will be followed and enforced for vehicles and equipment associated with the demolition project traversing Canal Road.

2.1 Mill Buildings and Ancillary Structures within the Project Area

The following sections describe the buildings and areas scheduled for demolition and abandonment as part of this project. This section provides summaries of the conditions assessed in the Site Development Assessment Report conducted by Fuss & O'Neill in August 2008 and the Strathmore Feasibility Study prepared in May 2005 by Finegold Alexander + Associates, Inc, FXM Associates, Tighe & Bond, et al. The conditions at the Project Site have continued to fall further into disrepair over the past twenty years due to limited maintenance and the infeasibility of redevelopment at the Project Site. The locations of each building are identified on Figures 1 and 2 in Attachment B. Representative site photographs are provided in Attachment C.

2.1.1 Building #1

Building #1 was constructed in 1877 and consists of a 5-story structure approximately 105 feet long by 43 feet wide. The building exterior is constructed of load-bearing brick masonry walls. The building floors are constructed of wood and are supported by wood beams, tension rods, and the exterior masonry walls, which include a combination of steel and wood framing and interior wood columns.

As part of the Site Development Assessment Report, a structural assessment of Building #1 was conducted to evaluate the fire damage caused to the building. The assessment indicates that the building is in fair condition, but identified several deficiencies in the west end of the building that may compromise the structural integrity of the building. The 5th floor received the most fire damage, followed by the 4th and 3rd floors. Additionally, this assessment found that the loadbearing walls in the southwest corner of the building have large cracks and that the wall sections around the cracks are moving outward, indicating severe structural damage.

The Site Development Assessment Report also identified two main deficiencies within Building #1 including: fire damage to the wood members within the building resulting in reduced structural integrity, and the mortar on the masonry joints is friable and easily scraped away. The Feasibility Study identified that the mortar joints on the exterior face of the building were deteriorating in many locations, and the moss growing on the brick in some areas indicated water was infiltrating the walls. This building is scheduled for demolition under Bid Alternate No. 1.

2.1.2 Courtyard (West)

The Courtyard (West) area measures roughly 83 feet by 73 feet and is surrounded by Buildings #1, #4, #11, and former Building #10. The Courtyard consists of primarily previously managed greenspace with a paved walkway that extends from Building #1 to the site of former Building #10. The Courtyard will be part of the demolition area; the concrete and pavement features are slated to remain.

2.1.3 Building #2

Building #2 was constructed in 1873 and consists of a 5-story structure approximately 160 feet long by 43 feet wide. The building exterior is constructed of load-bearing brick masonry walls. The building floors are constructed of wood and several are finished with a concrete surface. The floors are supported by the exterior masonry walls, which include a combination of steel and wood framing and interior steel lally columns.

The Site Development Assessment Report identified three main deficiencies within Building #2 including: weak story (a portion of the wall between building #2 and #3 was removed), deterioration of wood (the roof framing had fire damage), and mass (some floors consisted of wood planks, while others of concrete slabs).

The Feasibility Study identified that the mortar joints on the exterior face of the building were deteriorating in many locations and disintegrated to dust upon contact. In addition, evidence of water damage was documented in the wood ceilings of the third floor and roof beams. This building is scheduled for demolition under Bid Alternate No. 3.

2.1.4 Building #3

Building #3 was constructed in 1892 and consists of a 5-story structure approximately 176 feet long by 43 feet wide. The building exterior is constructed of load-bearing brick masonry walls. The building floors are constructed of wood and several are finished with a concrete surface. The floors are supported by the exterior masonry walls, which include a combination of steel and wood framing and interior steel lally columns. Building #3 contains empty concrete tanks, believed to historically hold water, and piping infrastructure.

The Site Development Assessment Report identified four main deficiencies within Building #3 including: interior mezzanine lacked bracing and adequate anchoring to the main structure, weak story (portions of the wall between Buildings #2 and #9 have been removed to create door openings over the years), soft story (the first floor was about twice as high as the remaining floors), and mass (some floors consisted of wood planks, while others of concrete slabs).

The Feasibility Study identified that the mortar joints on the exterior face of the building were deteriorating in many locations and disintegrated to dust upon contact. Moss observed growing on the brick in some areas indicated water was infiltrating the walls. In addition, evidence of water damage was documented in the wood ceilings of the third floor and roof beams. This building is scheduled for demolition under the Base Bid.

2.1.5 Loading Dock

The Loading Dock is located to the east of Buildings #3 and #7, and is elevated from the ground and supported by steel columns. An entrance between the Loading Dock and Building #3 connects these buildings. Neither the Site Development Assessment Report nor the Feasibility Study identified any deficiencies with this structure. This building is scheduled for demolition under the Base Bid.

2.1.6 Building #4

Building #4 was constructed in 1873 and consists of a 4-story structure approximately 95 feet long by 51 feet wide. The building exterior is constructed of load-bearing brick masonry walls, and the south exterior wall features covered walkways that span across the third and fourth floor of the building to Building #1 from ground level. The building floors are constructed of wood and several are finished with a concrete surface. The floors on the third floor are supported by the masonry walls, a combination of steel/wood framing and interior steel lally columns. The first and second floors are supported by steel beams encased in brick arches.

The Site Development Assessment Report identified four main deficiencies within Building #4 including: masonry joints (the mortar is soft, friable, and easily scraped away), deterioration (some of the wood framing was decayed or otherwise deteriorated), soft story (the first floor was about twice as high as the remaining floors), and mass (some floors consisted of wood planks, while others of concrete slabs). This building is scheduled for demolition under Bid Alternate No. 3.

2.1.7 Building #5/5A

Building #5/5A is a 5-story structure approximately 50 feet long by 38 feet wide. This building is among the oldest existing buildings within the Mill Complex and was constructed in the 1870s. The building walls consist of load-bearing brick masonry. The first floor of the building, which is located two stories below Canal Road, houses the boilers. The boilers extend from the first floor up to the third floor. Catwalks located above the first floor provide access between the boilers. The building does not have a second floor.

The building floors are constructed of wood and several are finished with a concrete surface. Steel beams encased in brick masonry arches support the third floor. Masonry walls, a combination of steel and wood framing and interior steel lally columns support the fourth floor. The fifth floor is supported by the masonry walls and wood framing. The fifth floor has additional floor supports consisting of tension rods that are evenly spaced throughout the length of the building. The rods on the fifth floor provide support for the floor and extend into the attic where they are attached to timber roof trusses. The Site Development Assessment Report identified one main deficiency within Building #5/5A: deterioration of wood. The Feasibility Study identified that the brick mortar joints were deteriorating and soft in many locations. This building is scheduled for demolition under the Base Bid.

2.1.8 Building #6/6A

Building #6/6A was constructed in 1892 and consists of a 4-story building approximately 50 feet long by 50 feet wide. The building walls consist of load-bearing brick masonry. The building floors are constructed of wood and are finished with a concrete surface. Steel beams encased in brick masonry arches support the third

floor. Masonry walls, a combination of steel and wood framing, and interior steel lally columns support the third floor. Building #6A also contains two 15,000-gallon oil tanks.

The Site Development Assessment Report identified two main deficiencies within Building #6/6a: deterioration of wood and deterioration of masonry joints. The Feasibility Study identified that the brick mortar joints were deteriorating and soft in many locations. This building is scheduled for demolition under the Base Bid.

2.1.9 Building #7

Building #7 was constructed in 1892 and consists of a 4-story building approximately 116 feet long by 56 feet wide. The building exterior is constructed of load-bearing brick masonry walls. The building floors are constructed of wood and are finished with a concrete surface. Steel beams encased in brick masonry arches support the second and third floors. Brick piers provide additional support for the third floor. The fourth floor is supported by the masonry walls, steel framing and interior steel lally columns. Building #7 contains multiple levels of concrete tanks, believed to historically hold water, and piping infrastructure.

The Site Development Assessment Report identified two main deficiencies within Building #7: the interior mezzanine was not braced or adequately anchored to the main structure, and masonry joints were deteriorated. The Feasibility Study identified that the brick mortar joints were deteriorating and soft on the north exterior wall and in the interior of the building. This building is scheduled for demolition under the Base Bid.

2.1.10 Building #8

Building #8 was constructed in 1873 and consists of a 2-story building approximately 74 feet long by 51 feet wide. Foundation walls along the east side of the building adjacent to the Power Canal were constructed of fieldstone masonry. The building floors are constructed of wood and wood topped with concrete. A combination of wood and steel trusses support the second floor. Steel cables are attached to the tops of the columns below the second floor along the west side of the building and are also attached to the first floor to provide structural support. This building also contains a below grade concrete water storage tank believed to historically hold water.

The Site Development Assessment Report identified two main deficiencies within Building #8: weak story (one side of the building has a concrete foundation wall resisting the lateral forces, and the other side of the building contains a brick exterior wall), and soft story (the floor heights vary). The Feasibility Study identified that Building #8 was in poor condition due to structural instability, water damage, and deterioration. It appeared steel cables were installed within the first floor to prevent the building from separating towards the Courtyard (East). The wood floor of the second floor was warped, likely from water damage, and the exterior brick mortar joints were deteriorating and soft in many locations. This building is scheduled for demolition under the Base Bid.

2.1.11 Building #9

Building #9 was constructed in 1909 and consists of a 4-story structure that is approximately 3,850 square feet. The building is located between and shares a wall with Buildings #2, #4, and #5/5A along Canal Road on a separate parcel of land identified as Parcel 02-0-01C (0.09 acres). This property is listed at 16 Canal Road and is currently owned by Turners Falls Creek Renewable Energy (dba Eagle Creek Renewable Energy). This occupied off-site building contains a sub-base level water wheel on the lowest floor with connecting penstock off the canal, with associated power generating equipment on the 2nd floor. The street-front 3rd floor level of this building contains the "power-house" and turbine, and this open floor spans up to the 2nd floor level.

Beneath the 1st floor level is the tailrace that crosses beneath Building #2 and discharges to the Connecticut River. This building is scheduled to remain.

2.1.12 Building #11

Building #11 was constructed circa 1900 and consists of a 7-story building approximately 105 feet long by 48 feet wide. Foundation walls along the east side of the building adjacent to the Power Canal were constructed of brick and fieldstone masonry. The exterior is constructed with multi-wythe brick masonry walls with windows spanning each facade across six stories. The building floors are constructed of wood or wood topped with concrete, supported by the masonry walls, wood beams, and/or brick columns.

The Site Development Assessment Report identified multiple deficiencies within Building #11: soft story (the floor heights vary) and masonry joints (mortar is soft and friable). The Feasibility Study identified that Building #11 was in poor condition due to structural instability, water damage, and deterioration. The wood flooring on the second floor had visible water damage, and the wood ceiling was visibly deteriorated in multiple locations. This building is scheduled for demolition under Bid Alternate No. 2.

2.1.13 Courtyard (East)

The Courtyard (East) area is surrounded by Buildings #3, #6, #7, and #8 and contains the base of the Smokestack that was removed in 2020. The base of the former Smokestack is deteriorating with brick masonry falling off the base. The Courtyard also contains a 9,000-gallon underground concrete tank believed to historically hold water. The tank is currently empty and is scheduled to be filled in with flowable concrete fill under the Base Bid.

2.1.14 Tailrace #3

Tailrace #3, located beneath Building #3, originates at the Power Canal and terminates at the riverside foundation wall along the Connecticut River. The Tailrace has an arch opening approximately 20 feet wide and 10 feet high; the area inside Tailrace #3 is approximately 20 feet across and 30 feet deep with 13 foot ceilings. Tailrace #3 has masonry and concrete construction with pipe penetrations on the back wall. Steel supports are in place for further structural stability. Debris (e.g., brick, driftwood) and silt have accumulated on the floor of the tailrace near the outlet due to water backing up from the Connecticut River. Steel I-beams on the ceiling are approximately three (3) feet on center.

Historically, water from the Power Canal flowed through the raceway, through the tailrace, and discharged to the Connecticut River. The inlet (penstock) was capped by the Town during the summer of 2018. The outlet remains open. Under the Base Bid, the tailrace is scheduled to be sealed at the outlet archway with a concrete wall and filled with flowable concrete fill beneath Building #3 and the Courtyard (East).

2.2 Asbestos and Hazardous Materials

Between 2014 and 2026, Tighe & Bond completed hazardous building material assessment (HBMA) surveys through the Mill complex buildings as part of earlier (limited) asbestos and hazardous building materials abatement work, and now in preparation for full building demolition. The presence of regulated asbestos-containing materials (RACMs) in several different materials throughout each building was confirmed. Materials containing asbestos include but are not limited to: pipe and boiler insulations, boiler packing and gaskets, pipe gaskets, transite cement panels and electrical boards, floor tiles, mastics, adhesives, window and door glazing and caulking, and built up roofing materials. Most of the safely accessible materials were removed from the interior portions of several of the buildings in 2020.

Section 2 | Existing Conditions

Hazardous materials identified within buildings include but are not limited to: fluorescent light tubes light, oil-filled ballasts, mercury containing switches and thermostats, emergency light batteries, fire extinguishers, containers of gear oils, containers of cleaning fluids, paints and solvents, and some areas of oil stained concrete surfaces. Some of these materials were removed from the interior portions of several of the buildings in 2020. Lastly, lead paint was identified in the buildings. If the Mill Complex were to catch fire or otherwise collapse, uncontrolled releases of hazardous material to the environment would occur.

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Section 3

SECTION 3 | Due Diligence Review

Tighe & Bond reviewed the *Massachusetts State Register of Historic Places* and conducted a review of the *Massachusetts Cultural Resource Information System* (MACRIS), National Register of Historic Places, and Sanborn Fire Insurance Maps relative to the proposed project area. Copies of the Sanborn Fire Insurance Maps are provided in Attachment D.

3.1 Strathmore Mill Complex

According to the MACRIS Inventory Form (MHC ID#: GIL.C / MNT.H), the existing buildings of the Strathmore Mill Complex were constructed between 1877 and the early 1900s for the Keith Paper Company. The Mill Complex was originally known as the Keith Paper Mill (MNT.131) and contains buildings contributing to the Turners Falls Historic District (MNT.H) which is listed in the National Register. Industrial use of the Project Locus (and surrounding area) pre-dates 1877, when the Keith Paper Mill Complex was destroyed by fire, but was rebuilt soon thereafter. According to the 1884 and 1889 Sanborn Fire Insurance Maps - copies of which are provided in Attachment D - the Keith Paper Company owned the structures that are currently identified as Buildings #1, #2, #4, #5/5A, #6/6A, #8, and #9. The land to the east (within the area of current Building #7, the Loading Dock, and the remainder of Parcel 02-0-06) contained buildings owned by the Montague Paper Company and John Russell Cutlery Company. Expansions of the Mill Complex occurred between 1893 and 1896, doubling the manufacturing production output.

The original power plant and associated structures were replaced with a more efficient hydroelectric plant in 1916. The Sanborn Fire Insurance Map for 1914, also provided in Attachment D, shows the expanded footprint of the Keith Paper Company as compared to 1889. The success of the Keith Paper Mill was attributed to availability of access to the following: railroad line, canal for raw material transport, canal water intakes for power generation, municipal water lines in Canal Road, and the [former] pedestrian bridge for workers. Between 1935 and 1953, more upgrades were installed. In 1953, the Keith Paper Company sold the complex to the Strathmore Paper Company of West Springfield, Massachusetts which continued operations until 1994. Between 1994 and 2007, the Mill Complex had multiple owners. In May 2007, an arson fire spread through several buildings within the Mill Complex. The fire led to the collapse of Building #10 and resulted in damage to adjacent Buildings #1 and #11.

Due to the deterioration of the structures, the limitation in reuse, and the liability associated with the attractive nuisance of these structures, the Town is proposing the controlled demolition of structures within the Mill Complex.

3.2 M.G.L. Chapter 9 Sections 26-27C Compliance

Based on a review of the Massachusetts Cultural Resource Information System (MACRIS), the Mill Complex lies within one historic district listed on the National Register of Historic Places (NR), the Turners Falls Historic District (MNT.H). The Mill Complex is also within the limits of two historic districts not listed on the NR or State Register: the Riverside District (MNT.G) and the Turner Falls Power and Electric Company Historic District (MNT.K). A figure showing the historic districts and inventoried properties near the Project Site is provided on Figure 3 in Attachment B. Table 3-1 below lists the results of MACRIS inventoried properties and areas within the project area.

3.2.1 Historic District/Area

The Mill Complex is located within the Turners Falls Historic District (National Register of Historic Places, Reference No. 82004966; MHC Inventory No. MNT.H) certified on May 2, 1982. This District is roughly bounded by the Connecticut River to the west, First Street to the north, Prospect Street and Ninth Street to the south/east, and L Street to the south within the Village of Turners Falls. According to the MHC inventory form, 250 Buildings, 4 sites, and 6 structures contribute to the Turners Falls Historic District.

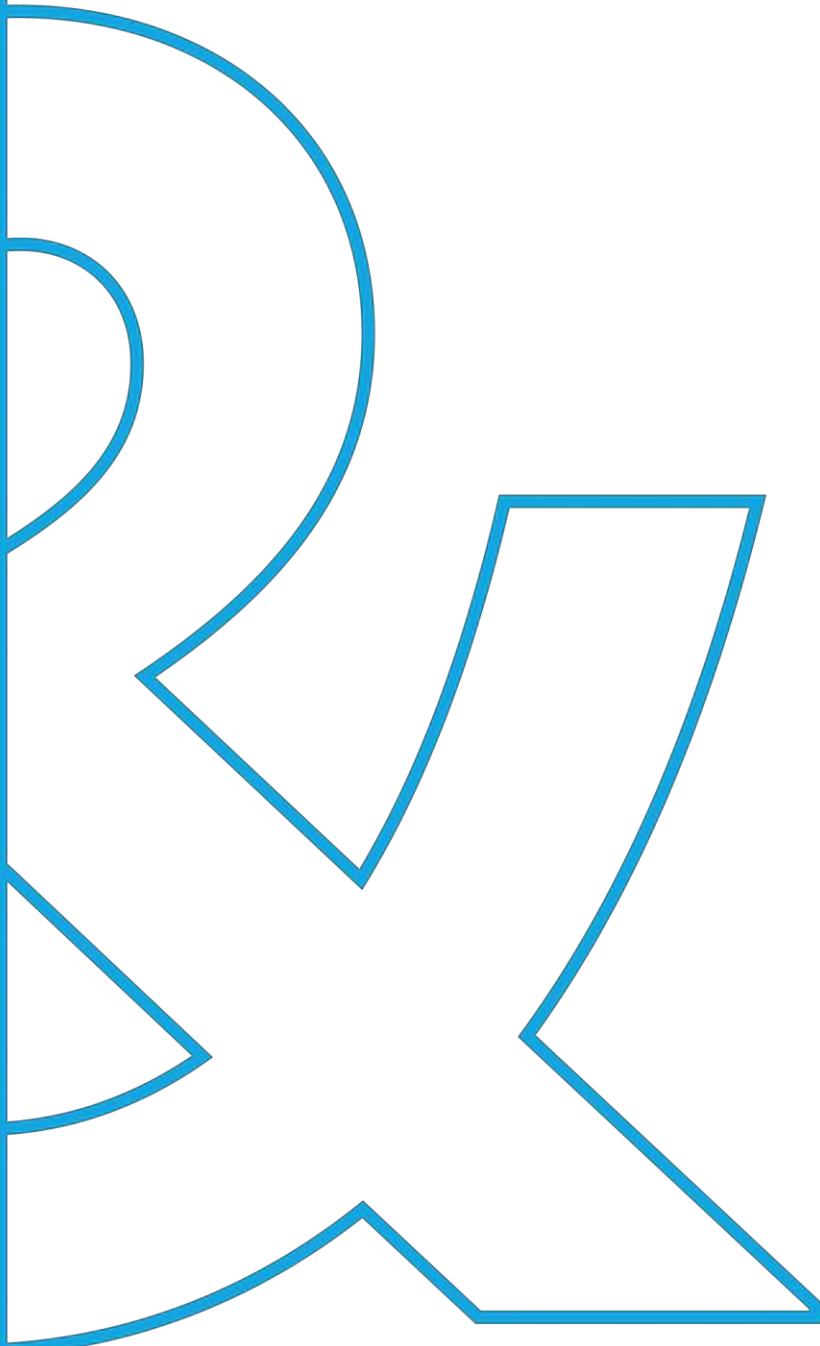
TABLE 3-1 Summary of MACRIS-listed Cultural/Historical Assets within the Project Area

MACRIS No.	Historic Name	Designation	Significance
MNT.G	Riverside	--	Architecture; Community Planning; Engineering; Industry; Transportation
MNT.H	Turners Falls Historic District	National Register District	Archaeology, Historic; Architecture; Commerce; Community Planning; Engineering; Industry; Recreation; Social History
MNT.K	Turners Falls Power and Electric Company	--	Architecture; Engineering; Industry
MNT.131 ¹	Keith Paper Mill	Contributing Building to Turners Falls National Historic District	Architecture; Engineering; Industry

¹This listing is located within the Historic Riverside Area and Turners Falls National Historic District.

3.2.2 Inventoried Property

The Strathmore Mill Complex is an MHC-inventoried property (MHC, Inventory No. MNT.131) noted for its architectural, engineering, and industrial significance. This complex was generally used as an industrial complex with an edged tool factory and paper mill. It was formerly referred to as both the Keith Paper Mill and Hammermill Paper Company. This property is located within the Historic Riverside Area and is a contributing building to the Turners Falls National Historic District. The Keith Mill Footbridge over the Power Canal (MHC, Inventory No. MNT.925) was located adjacent to the proposed project and was demolished in 2025 as part of a separate project by others.



Section 4

SECTION 4 | Proposed Project

4.1 Proposed Activities

4.1.1 Prior to Demolition

ASBESTOS AND HAZARDOUS MATERIALS ABATEMENT

As previously mentioned, asbestos-containing materials and hazardous materials have been identified within buildings scheduled for demolition. Regulated asbestos-containing materials will be abated prior to or during demolition. Hazardous materials will be abated prior to demolition and areas of oil-stained concrete surfaces will be segregated for disposal during demolition.

Lead paint management will be subject to OSHA 1926.62 Lead in Construction regulations, which require the contractor to adequately protect their workers from exposure to airborne lead. It is expected that employee exposure to lead will be minimal as the majority of demolition work will involve the use of mechanical equipment. However, in the event selective demolition procedures include the use of torches or reciprocating saws where lead paint is present, those surfaces would be abated prior to the disturbance to reduce potential worker exposure.

WOODY VEGETATION REMOVAL

Approximately 12,500 sf of vegetation clearing and grubbing is proposed within the vicinity of the Loading Dock area to facilitate access for demolition activities. Mechanical vegetation removal methods will be used for this task. Vegetation will be removed from the site for proper disposal. A significant portion of this area is currently armored with rip rap; the final ground treatment in these areas will remain rip rap.

4.1.2 Demolition

Demolition will include a combination of Buildings #1, #2, #3, #4, #5/5A, #6/6A, #7, #8, #11, the base of the Smokestack, pipe structures within each building, and the Loading Dock attached to Building #3. The buildings to be demolished are dependent upon the demolition bids that the Town receives, as outlined in Table 1-1 in Section 1 above. Above-grade portions of buildings will be demolished to the lowest slab elevation; the slab and foundation walls will remain intact. The total area of demolition for the Proposed Project and its potential alternates is approximately 71,100 square feet.

Demolition is proposed to the lowest slab elevation; the remaining slabs will be perforated for drainage and foundation walls will remain intact. Within Courtyard (East), the base of the Smokestack will be demolished, and the 9,000-gallon holding tank slab will be filled-in with flowable concrete fill. Utilities serving the buildings to be demolished will be terminated, cut, plugged and/or capped.

It is anticipated that work will be undertaken with the use of typical construction equipment including cranes, excavators, grapplers, front-end loaders, waste-trailers, and dump trucks. Demolition debris will be temporarily stockpiled as it is generated, placed on the first-floor concrete slabs, and segregated for proper off-site disposal. Dust from construction activities will be controlled via the use of water sprayers or spray guns.

Based on site constraints, it is anticipated that demolition will occur from east to west starting at the Loading Dock. The buildings/areas will generally be demolished/abandoned in the following order: Loading Dock, Building #7, Building #8, base of the Smokestack, Courtyard (East) holding tank (to be abandoned in place), Building #6/6A, Building #3, Tailrace #3 (capping and fill), Building #5/5A under the Base Bid. If there is

funding to incorporate bid alternates, the buildings and structures to be demolished are outlined in Table 1-1 in Section 1. 1. Stockpiling details are discussed below.

STOCKPILING

Demolition debris will be stockpiled initially on the first-floor concrete foundations for segregation and subsequent transport from the site. Designated longer-term stockpiling areas will be established for materials to be salvaged (e.g., scrap metal, intact wood beams, etc.). Brick and concrete materials that are scheduled to be crushed and re-used on-site will also be stockpiled on concrete foundations and within a designated stockpiling location. Stockpiles that are not on concrete foundations will be surrounded by perimeter controls to minimize the potential for materials to migrate off-site. Material that will not be re-used on-site will be stockpiled on concrete foundations prior to transport for disposal.

MATERIAL UTILIZATION, BACKFILLING, AND GRADING

Due to the depths of the lowest building slabs, the project will entail backfilling to create a slope from Canal Road to the lowest elevation of the lowest slab. This will serve to stabilize the remaining building foundation walls along Canal Street, and along the east side (and west, if Building #4 is demolished) of Building #9 to remain.

Processed (crushed to less than 3 inches) brick/concrete debris will be generated during the scheduled building demolition. The materials consist of brick wall materials and concrete floors. The brick/concrete materials to be reused as structural backfill material will account for a significant percentage of material required for post-demolition backfill and grading. Since portions of the brick are coated with paint, a Beneficial Use Determination (BUD) will be submitted to the Massachusetts Department of Environmental Protection (MassDEP) for review and approval prior to reuse of these materials on-site. Therefore, the re-use of these materials on site will significantly limit the amount of materials to be disposed off-site and the volume of backfill needed. Correspondingly, it will reduce construction-related traffic to and from the Project Area.

The difference in elevation between Canal Road and the first-floor level of the Mill Complex is approximately 24 feet. Backfill will be placed at a 3:1 slope (or less) toward the south side of Canal Road. The material required to achieve proposed grades is dependent upon the Demolition Alternate that is ultimately selected. The proposed area for the beneficial use of the processed materials is at the lower levels within this sloped area, beginning against the fieldstone masonry walls to remain intact along/abutting the Canal Road. As shown on the attached project drawings, the footprints of demolished Buildings #5/5A, #6/6A, #7 and #8, and the East Courtyard area constitute the primary backfilling area; if Buildings #4 and 11 are demolished, additional backfilling will be conducted in their respective footprints (i.e, the secondary areas). Additional details of materials reuse and site stabilization are outlined below:

- The ground floor slabs will be punctured for drainage purposes and a geotextile filter fabric will be placed atop the punctured floor. Processed material will be stockpiled on geotextile filter fabric. The lowest level building slabs to remain will be perforated for drainage.
- The stockpiled processed materials will be placed in uniform horizontal layers not to exceed 12 inches in thickness and compacted with heavy self-propelled vibratory compaction equipment (e.g., vibratory rollers).
- The processed materials will be placed at a 3:1 (or less) slope.
- Following the completion of placing the processed materials, a geotextile filter fabric layer will be placed atop these materials along with a visual warning barrier (e.g., orange snow fence material). A minimum of three (3) feet of clean, off-site gravel borrow will then be placed over these areas.
- For the primary backfill/sloped area, the surface treatment of the sloped area will consist of 2.5 feet of clean off-site gravel borrow and 0.5 feet of loam to be broadcast with grass seed to establish vegetative

cover. For the secondary backfilled/sloped areas, the surface of the 3-foot cover will be rip rap material.

4.1.3 Tailrace #3 Closure

Closure of Tailrace #3 will be achieved through the construction of an engineered barrier (referred to herein as the cap) at the tailrace outlet, flush with the exterior face of the riverside foundation wall. As previously mentioned, the inlet (penstock) was capped by the Town during the summer of 2018. In order to cap the tailrace with minimal impact to the nearby Bank of the Connecticut River, access will be gained through the remaining floor of Building #3 following demolition of the upper portions of the building. This will likely be accomplished by the use of a "snooper" type truck with a boom lift to lower laborers and equipment into the tailrace for debris removal and concrete formwork.

Tailrace #3 closure will include the following activities:

- Loose bricks, driftwood, and other debris will be removed from within the tailrace and at the outlet.
- A 2-foot diameter CMU or concrete wall will be constructed at the entrance to the tailrace.
- Silt fence and erosion control barriers will be placed just outside the outlet of the tailrace.
- Debris will be stockpiled on the building foundation for offsite disposal.
- No debris will be placed or stockpiled within the Connecticut River or upon its Bank.
- Silt deposits will be removed down to sound soil to reduce the potential for cap settlement.
- Silt debris will be placed in the interior of the tailrace.

The engineered cap will be constructed of reinforced concrete followed by filling the remaining voids with controlled density fill finished flush with the top of the tailrace.

4.1.4 Post-Demolition Site Restoration

The majority of the project is located within previously developed areas (i.e., the Mill Complex). Site restoration will include loaming and seeding of disturbed previously vegetated areas, where current rip rap materials do not exist, and new slope construction as discussed above. Site restoration also includes new wall construction around Building #9, as reviewed in Table 1-1 in Section 1.

4.2 Construction Access

Access to the Project Area is limited to a single roadway, Canal Road. The site will be accessed from the Turners Falls Road bridge over the Connecticut River. Contractors will be required to abide by height, weight, and width restrictions for Canal Road.

4.3 Construction Period Protective Measures

Typical mitigation for stormwater runoff and associated erosion and sedimentation, fugitive dust, vehicle emissions, and soils and solid waste management will be implemented for the duration of the demolition period of the Project. There are no visible stormwater collection structures noted within the limits of the work. If identified, inlet structures will be protected with silt sacks or similarly effective devices. The following protective measures will be implemented during construction to minimize the potential for erosion and sedimentation.

4.3.1 Erosion Control Barriers

Prior to demolition, the limits of work will be marked with perimeter barriers and erosion controls. The top of the concrete wall of the Power Canal adjacent to Canal Road provides a vertical, impervious barrier that protects the Power Canal. No additional measures are proposed at this location.

The Connecticut River will be protected by a row of erosion control barriers at the limits of work near the Loading Dock. The erosion control barriers will consist of silt fencing, Filtrexx Siltsoxx, or other similarly effective devices depending on the location. In addition:

- The Contractor will be required to maintain a reserve supply of erosion control barriers on-site to make repairs, as necessary.
- Protective measures will be inspected after significant precipitation events. Maintenance and repairs will be conducted, as necessary.

At the conclusion of the project, the erosion control barriers will be removed and properly disposed off-site following the stabilization of disturbed areas.

4.3.2 Debris Collection Netting

The demolition of Buildings #1, #2, and #3 are proposed immediately adjacent to the Connecticut River. Additional protective measures consisting of netting to catch falling debris will be implemented to minimize the potential of incidental deposition into the river. Prior to demolition, a netting system will be designed, installed and maintained by the demolition contractor.

It is expected that the barrier system will consist of a series of steel beams secured horizontally to the buildings' walls. The steel beams will extend through lower window openings of the north building wall, approximately 10 feet or more from the building. Strong netting or similar material will then be draped over and secured to the beams. Careful demolition is planned in this area and the north building wall will be pulled inward in a southerly direction, away from the river. The demolition will proceed using a bay-by-bay method, starting from the east and moving to the west. This netting is intended to catch loose bricks or concrete pieces that may fall in the direction of the Connecticut River (to the north). This type of system has been effectively implemented on similar projects. In the event that loose material falls onto the shoreline of the Connecticut River, the contractors will be required to remove the material.

The edge of the Connecticut River will not be disturbed or altered. Existing brick and mortar debris scattered along the river edge at the beginning of the project will remain.

4.3.3 Power Canal Protection

The top of the concrete wall of the Power Canal adjacent to Canal Road provides a vertical, impervious barrier that protects the Power Canal. No additional measures are proposed at this location.

4.3.4 Proposed Handling and Best Management Practices of Beneficial Reuse Material

The following Best Management Practices (BMPs) will be implemented during the handling and processing of the beneficial reuse material:

- During building demolition, materials with significant oil staining of the coated and uncoated surfaces will be segregated for proper off-site disposal.
- During building demolition, the coated and uncoated materials, as defined herein, will be segregated and stockpiled separately, to the extent feasible, at one or more locations on-site.
- Bricks that are intact and in good shape will be stockpiled for use as accents in the Building 9 façade.

Section 4 | Proposed Project

- Dust from the demolition activities, stockpiled brick/concrete rubble, crushing operations and crushed material will be monitored through a visual standard of “no visible dust emissions” and controlled with water sprayers. Following crushing, the materials will be used as backfill and compacted in a pre-determined location.

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Section 5

SECTION 5 | Pertinent Regulatory Programs

The proposed activities will require permits from the following local, state, and federal agencies as indicated in Table 5-1.

TABLE 5-1 Summary of Regulatory Permits and Authorizations

Regulatory Agency	Statute/Authority	Permit/Authorization
Montague Conservation Commission	M.G.L. c. 131 §40 / 310 CMR 10.00	Order of Conditions
Montague Historical Commission	Montague Town Bylaw / Article 35 / Demolition Delay Bylaw	Determination of Effect
Massachusetts Historic Commission	M.G.L. c. 9 §§26-27C / 950 CMR 71.00	Determination of Effect
Massachusetts Department of Environmental Protection (MassDEP)	M.G.L. c. 21A § 2 & § 8	Beneficial Use Determination
Massachusetts Wildlife Natural Heritage & Endangered Species Program (NHESP)	M.G.L. c. 111 §150A, §150A1/2 / 310 CMR 19.00	Determination of Effect/Determination of Take

5.2 Local Regulations

5.2.1 Montague Demolition Delay Bylaw

The Montague Demolition Delay Bylaw was adopted in May 2022 and aims to protect significant buildings within the Town of Montague. The bylaw introduces a demolition permit process and timeline that is overseen by the Montague Inspector of Buildings. The Town has undertaken an extensive multi-year planning process regarding the Strathmore Mill Demolition Project that includes participation from the public. The Town has coordinated with the Montague Historical Commission and Montague Inspector of Buildings; given the immediate threat to public health and safety due to the mill complex's deteriorated condition, the Town is petitioning for emergency demolition per Section 5 of the Demolition Delay Bylaw.

5.3 Project Funding

The Project will receive nearly \$10M in state and federal funding for this project. The Town of Montague will receive \$5M for this project from the Massachusetts Department of Conservation and Recreation for the purpose of addressing environmental and public safety risks associated with the Strathmore Mill Complex. This funding was earmarked in the Massachusetts 2018 state environmental bond bill. Additionally, the US Environmental Protection Agency allocated \$4.9M in federal grant funding to the project through the Brownfields Cleanup Grant, which is funded by the Bipartisan Infrastructure Law (Infrastructure Investment and Jobs Act) passed in November 2021.



Section 6

SECTION 6 | Summary

The Mill Complex poses a health and safety threat to trespassers and/or first responders that may be required to access the property, which is primarily vacant. There are multiple challenges complicating site redevelopment, including: deteriorating buildings, limited site access and parking, the cost for rehabilitation is not supported by market rates, outdated infrastructure, and legal issues. Due to the deterioration of the structures, the limitation in reuse, and the liability associated with the attractive nuisance of these structures, the Town is proposing the controlled demolition of structures within the Mill Complex.

The Town anticipates that the demolition project will enable the Town to remove an attractive nuisance and develop riverfront public open space that honors the site's industrial history. Therefore, it is in the best interest of the Town to proceed with the demolition to protect the residents of Montague and adjacent environmental resources, and to provide an open space amenity and riverfront access.

In recognition of the historical importance of the site, the project has incorporated the preservation of certain building foundation elements, as well as incorporating existing bricks into a new brick facade for new outer walls for Building #9.

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**Attachment A:
Project Notification Form**



APPENDIX A
MASSACHUSETTS HISTORICAL COMMISSION
221 MORRISSEY BOULEVARD
BOSTON, MASS. 02125
617-727-84, FAX: 617-727-5128

Project Name: Strathmore Mill Complex Demolition Project

Location / Address: Canal Road

City / Town: Montague

Project Proponent

Name: Town of Montague, ATTN: Walter Ramsey

Address: 1 Avenue A

City/Town/Zip/Telephone: Turners Falls, MA 01376 / p: (413) 863-3200

Agency license or funding for the project (list all licenses, permits, approvals, grants or other entitlements being sought from state and federal agencies.)

Agency Name

Type of License or funding (specify)

Montague Conservation Commission

MAWPA Order of Conditions

Montague Historical Commission

Determination of Effect

Massachusetts Historical Commission

Determination of Effect

Massachusetts Department of Environmental Protection

Beneficial Use Determination

MassWildlife Natural Heritage & Endangered Species Program

MESA Determination of Effect / Determination of Take

Massachusetts Department of Conservation and Recreation

Funding from MA Environmental Bond Bill (2018)

Environmental Protection Agency

Funding from Brownfields Cleanup Grant

Project Description (narrative):

The proposed project entails asbestos abatement and demolition of eleven (11) buildings and ancillary structures associated with the Strathmore Mill Complex on Canal Road in Montague, MA. The Mill Complex consists of 11 interconnected brick buildings ranging in height from two to seven stories with a total floor area of 245,000 gross square feet. A loading dock and building associated with Mill Complex are located on an adjacent parcel (Parcel 02-0-06) to the east at 8 Canal Road. No new construction is proposed. Land use throughout the area includes commercial, industrial, waterways, and paved roadways. Construction access will use existing paved roads.

Does the project include demolition? If so, specify nature of demolition and describe the building(s) which are proposed for demolition.

The proposed project is the demolition of the eleven (11) buildings and ancillary structures within the Strathmore Mill Complex, which include Buildings #1, #2, #3, #4, #5/5A, #6/6A, #7, #8, and #11, the base of the Smokestack located in Courtyard (East), and the Loading Dock attached to Building #3. Prior to demolition, the limits of work will be marked with perimeter barriers and erosion controls. The total area of demolition is approximately 71,100 square feet. Please see the cover letter for a detailed description of each building and ancillary structure that is scheduled for demolition.

Once demolition is completed, the erosion control barriers will be removed and properly disposed off-site following the stabilization of disturbed areas and Conservation Commission authorization. Site restoration will include loaming and seeding of disturbed previously vegetated areas and sloped areas. Demolished building footprints and retaining walls will either be incorporated into the newly sloped areas or remain as crushed concrete and brick. The area of Loading Dock demolition will be restored to existing grade and also loamed and seeded.

APPENDIX A (continued)

Does the project include rehabilitation of any existing buildings? If so, specify nature of rehabilitation and describe the building(s) which are proposed for rehabilitation.

The removal of the buildings surrounding Building #9 will result in the need to construct substantial reinforced concrete and concrete masonry unit (CMU) walls to resist the force of exterior loading. These reinforced concrete retaining walls will be constructed along northern side of Building #9. Intact bricks from the demolition will be segregated for use as accents in the new exterior wall of Building #9. Additionally, a new stair tower would need to be constructed to supplement required egress out of the building for Bid Alternate #3. No further building rehabilitation is proposed.

Does the project include new construction? If so, describe (attach plans and elevation if necessary).

If Bid Alternative No. 3 moves forward, a new stair tower is anticipated at Building #9. The new stair tower footprint will be minimal.

To the best of your knowledge, are any historic or archaeological properties known to exist within the project's area of potential impact? If so, specify.

Based on a review of the Massachusetts Cultural Resource Information System (MACRIS), the Mill Complex lies within one historic district listed on the National Register of Historic Places (NR), the Turners Falls Historic District (MNT.H). The Mill Complex is also within the limits of two historic districts not listed on the NR or State Register, the Riverside District (MNT.G), and the Turner Falls Power and Electric Company Historic District (MNT.K).

The Keith Mill Footbridge over the Power Canal (MHC Inventory No. MNT.925) was located adjacent to the proposed project and has been demolished as part of a separate project by others. Refer to the cover letter and the Historic Sites figure (Figure 3) included in Attachment B.

What is the total acreage of the project area?

Woodland	<u>0.05</u> Acres	Productive Resources	<u>0</u> Acres
Wetland	<u>0</u> Acres	Agriculture	<u>0</u> Acres
Floodplain	<u>0</u> Acres	Forestry	<u>0</u> Acres
Open space	<u>0</u> Acres	Mining/Extraction	<u>0</u> Acres
Developed	<u>2.12</u> Acres	Total Project Acreage	<u>2.17</u> Acres

What is the acreage of the proposed new construction?

The new stair tower at Building #9 will occupy approximately sf of land.

What is the present land use of the project area?

The Project Area lies within previously developed industrial areas associated with the former Strathmore Paper Mill Complex along Canal Road on the northern bank of the canal. The project area is currently abandoned, and the buildings scheduled for demolition are vacant.

Please attach a copy of the section of the USGS quadrangle map which clearly marks the project location.

This Project Notification Form has been submitted to the MHC in compliance with 950 CMP 71.00.

Signature of Person submitting this form: _____ Date: _____

Name: Tracy J. Adamski, Tighe & Bond, Inc.

Address: 53 Southampton Road

City/Town/Zip: Westfield, MA 01085

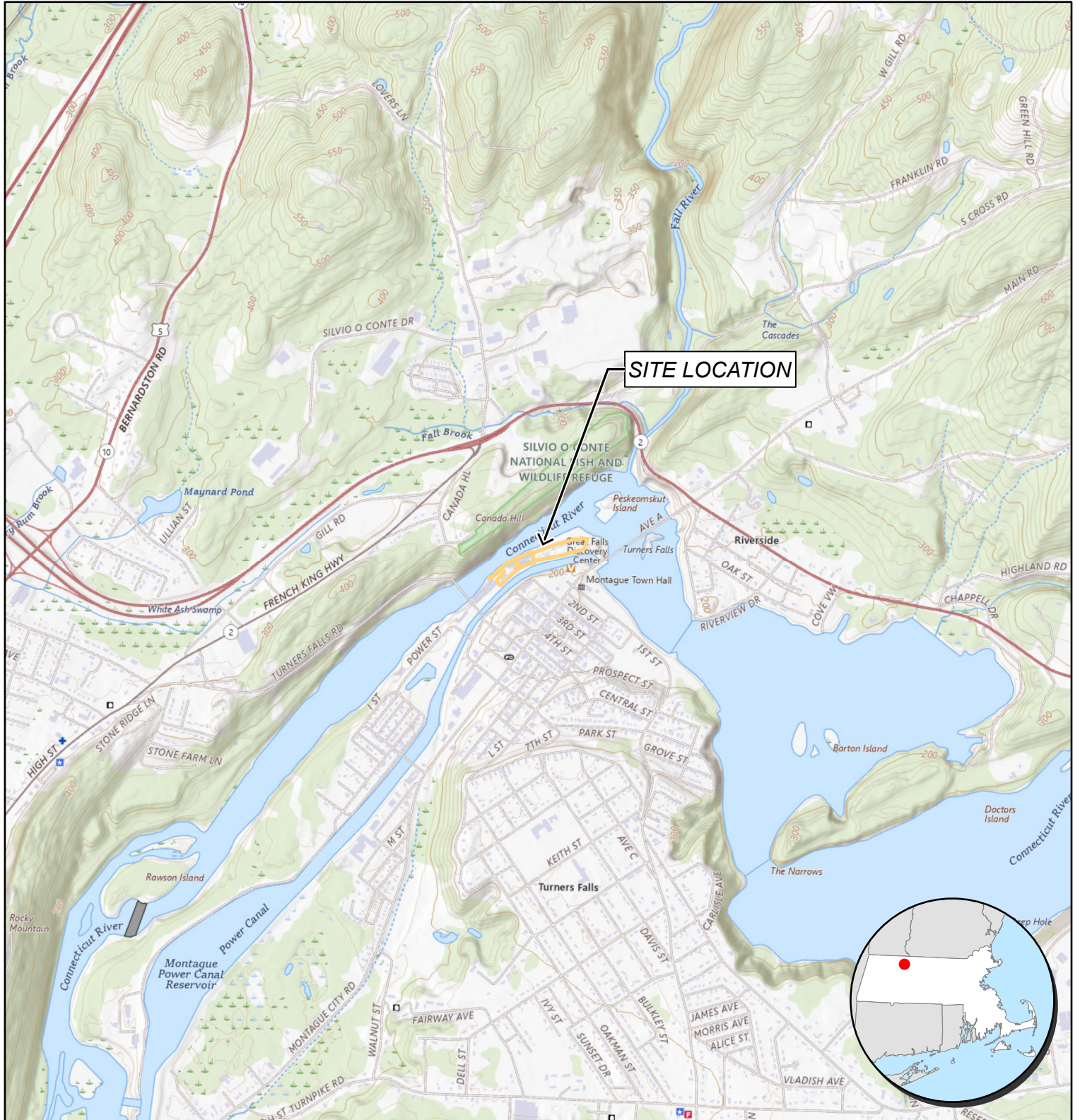
Telephone: (413) 572-3256

REGULATORY AUTHORITY

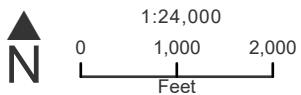
950 CMR 71.00: M.G.L. c. 9, §§ 26-27C as amended by St. 1988, c. 254.

**Attachment B:
Figures**

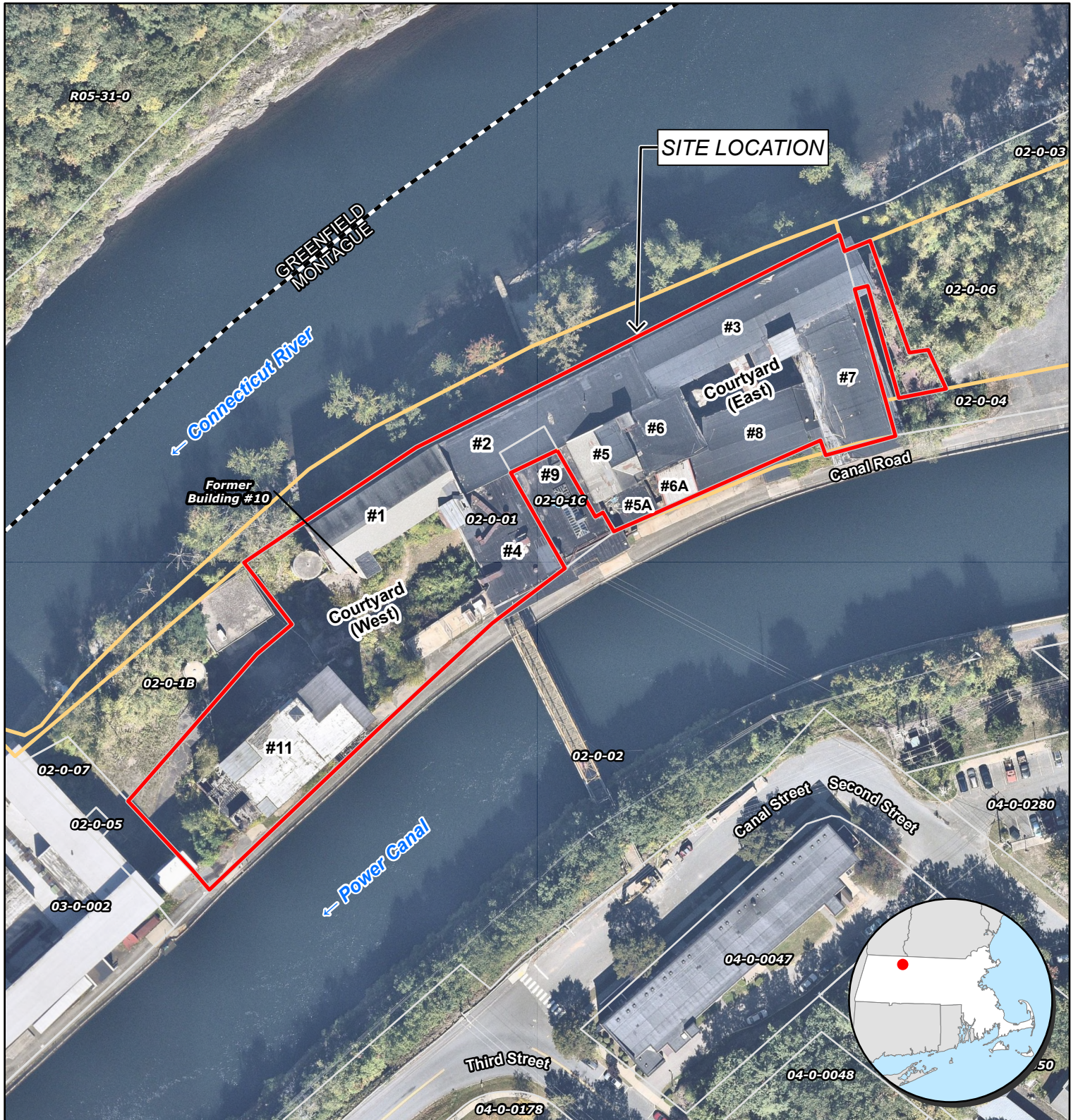




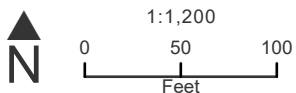
— Project Locus



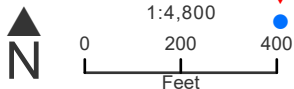
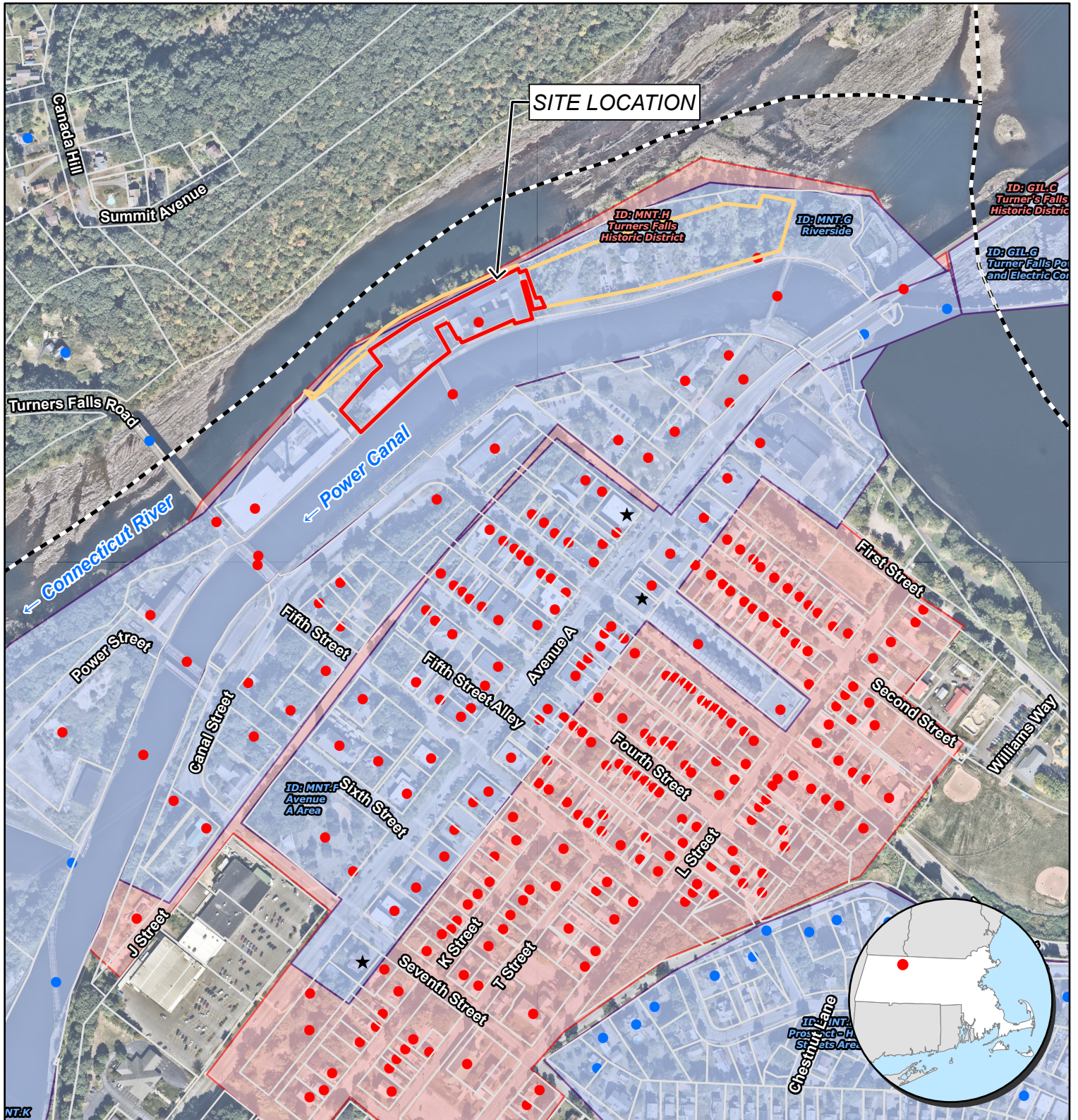
Based on USGS The National Map Topo Basemap.
Contour Interval Equals 10 Feet.



- Project Locus
- Limit of Demolition
- Municipal Boundary
- Approximate Parcel Boundary



Based on latest Nearmap Imagery (September 20, 2025).
 Parcel boundaries were provided by MassGIS and are approximate.

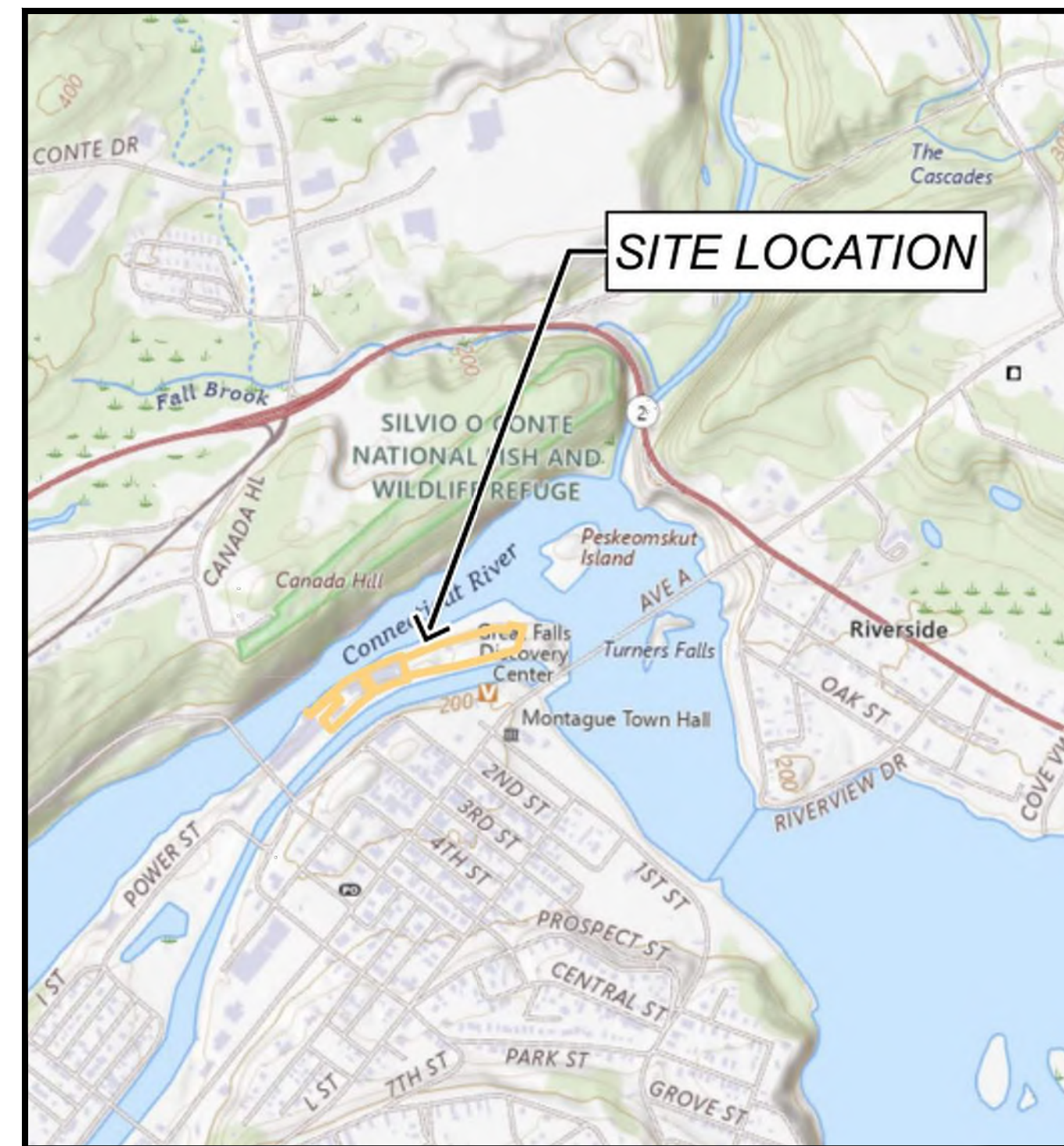


Based on latest Nearmap Imagery (September 20, 2025).
 MHC Inventory data downloaded from MassGIS.
 Parcel boundaries were provided by MassGIS and are approximate.

FORMER STRATHMORE MILL ABATEMENT, DEMOLITION AND RESTORATION PROJECT TURNER FALLS, MASSACHUSETTS

PERMIT SET MAY 2026

LIST OF DRAWINGS		
SHEET NO.	DRAWING NO.	DRAWING TITLE
GENERAL		
1	G-001	COVER SHEET
2	G-002	GENERAL NOTES & LEGEND
CIVIL		
3	C-100	EXISTING CONDITIONS & LIMITS OF WORK AREA PLAN
4	C-200	DEMOLITION PLAN
5	C-300	HISTORICAL PRESERVATION PLAN
6	C-400	SITE RESTORATION & GRADING PLAN
7	C-500	SITE DETAILS
STRUCTURAL		
8	S-100	TAILRACE #3 CLOSURE DETAILS
9	S-301	STRUCTURAL WALL ELEVATIONS 1 - HISTORICAL
10	S-302	STRUCTURAL WALL ELEVATIONS 2 - HISTORICAL
11	S-303	STRUCTURAL WALL ELEVATIONS 3 - HISTORICAL
12	S-502	STRUCTURAL DETAILS - HISTORICAL



SITE LOCATION MAP
SCALE: 1" = 1000'

PREPARED BY:

Tighe & Bond

PREPARED FOR:

CLIENT/MUNICIPALITY
THE TOWN OF MONTAGUE

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IT IS NOT INTENDED FOR BIDDING OR
CONSTRUCTION PURPOSES.

COMPLETE SET 12 SHEETS

GENERAL UTILITY NOTES (PERMIT LEVEL):

- NOTIFY "DIG SAFE" AT 1-888-344-7233 OR 811 TO ARRANGE FOR MARKING OUT EXISTING UNDERGROUND UTILITIES AT LEAST 72 HOURS IN ADVANCE OF MAKING EXCAVATION AT ANY GIVEN LOCATION. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE ALLOWED TO START ANY KIND OF EXCAVATION WORK PRIOR TO HIS OBTAINING ALL THE NECESSARY INFORMATION REGARDING THE LOCATION OF UNDERGROUND UTILITIES AT THE SITE.
- THE LOCATION OF THE UTILITIES AS SHOWN HEREON HAVE BEEN COMPILED FROM VISIBLE STRUCTURES AND INFORMATION OBTAINED FROM VARIOUS SOURCES. THE ACTUAL LOCATION OF ALL UTILITIES AND UNDERGROUND STRUCTURES SHALL BE CONSIDERED APPROXIMATE AND SHALL BE VERIFIED BY THE UTILITY OWNER PRIOR TO ANY CONSTRUCTION. THE ENGINEER MAKES NO GUARANTEES THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICES OR ABANDONED. THE ENGINEER EITHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED.
- FIELD MEASURE TO VERIFY EXISTING AND CONTRACT INTERFACE DIMENSIONS, LOCATIONS, AND OTHER CONDITIONS.
- ACCOMPLISH ALL EXCAVATION SO THAT UNDERGROUND UTILITIES OR STRUCTURES ARE NOT DAMAGED. BE RESPONSIBLE FOR ANY DAMAGE INCURRED DURING EXCAVATION OPERATIONS. REPAIR ANY EXISTING PIPE OR UTILITY DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
- TAKE ALL NECESSARY MEASURES AND PROVIDE ALL NECESSARY CONTINUOUS BARRIERS OF SUFFICIENT TYPE, SIZE, AND STRENGTH TO PREVENT ACCESS TO ALL OPEN EXCAVATIONS AT THE COMPLETION OF EACH DAY'S WORK.
- NOTIFY THE OWNER AND THE ENGINEER OF ANY STORM, SANITARY, OR OTHER PIPE DISCOVERED DURING CONSTRUCTION THAT IS NOT SHOWN ON THE DRAWINGS.

ENVIRONMENTAL REQUIREMENTS (PERMIT LEVEL):

- PROVIDE AND MAINTAIN PROPER SEDIMENT AND SOIL EROSION CONTROL DEVICES AROUND ALL CONSTRUCTION ACTIVITIES THROUGHOUT THE DURATION OF THE ENTIRE PROJECT, AS REQUIRED BY THESE CONTRACT DOCUMENTS AND SHOWN ON CONTRACT DRAWINGS.
- PROVIDE DUST CONTROL USING WATER DURING CRUSHING OPERATIONS AND FOR ALL RAW SLOPES, ALL EARTH STOCKPILES, ANY EARTH PILED ALONG EXCAVATIONS, AND SURFACES OF REFILLED TRENCHES. IN NO CASE SHALL EXCAVATED MATERIAL BE STOCKPILED AT SUCH LOCATIONS WHERE THE MATERIAL CAN RUNOFF INTO WETLAND AREAS, WATERWAYS, OR ADJACENT PROPERTIES.
- RESTORE AREAS DISTURBED BY CONSTRUCTION TO PRE-CONSTRUCTION CONDITIONS.
- PROVIDE FILTER FABRIC WRAP EROSION PROTECTION AROUND ALL CATCH BASINS WITHIN THE WORK AREA AND AS OTHERWISE SHOWN ON THE DRAWINGS. SEDIMENT TRAPPING DEVICES SHALL BE SILTSACK®, DANDY BAG II®, OR EQUAL.
- CLEAN ALL ROADWAY AND ADJOINING AREAS AFFECTED BY THE PROPOSED WORK TO AVOID SEDIMENT BUILD-UP AND TO MINIMIZE DUST GENERATION.
- STORE FUEL, OIL, PAINT, OR OTHER HAZARDOUS MATERIALS IN A SECONDARY CONTAINER AND REMOVE FROM THE SITE TO A LOCKED INDOOR AREA WITH AN IMPERVIOUS FLOOR DURING NON-WORK HOURS.
- PROVIDE A SUPPLY OF ABSORBENT SPILL RESPONSE MATERIALS, SUCH AS BOOMS OR BLANKETS, AT THE CONSTRUCTION SITES AT ALL TIMES TO CLEAN UP POTENTIAL SPILLS OF HAZARDOUS MATERIALS.
- IMMEDIATELY REPORT SPILLS OF OIL AND/OR HAZARDOUS MATERIALS (OHM) TO THE MASSACHUSETTS DEPARTMENT OF ENVIRONMENTAL PROTECTION.

ASBESTOS ABATEMENT NOTES (PERMIT LEVEL):

- REFER TO SPECIFICATION SECTION 13281 AND ABATEMENT SCHEDULE FOR COMPLETE DETAILS AND QUANTITIES TO BE REMOVED.
- ASBESTOS CONTAINING MATERIALS ARE NOT IDENTIFIED ON DRAWINGS.
- BUILDINGS SUBJECT TO VANDALISM AND WATER DAMAGE AND MAY BE STRUCTURALLY UNSOUND.

OIL/HAZARDOUS MATERIAL NOTES (PERMIT LEVEL):

- REFER TO SPECIFICATION SECTION 13283 FOR GENERAL IDENTIFICATION AND APPROXIMATE QUANTITIES OF HAZARDOUS MATERIALS TO BE REMOVED.

DEMOLITION NOTES (PERMIT LEVEL):

- DRAWINGS WERE DEVELOPED FROM RECORD DRAWINGS AND SITE OBSERVATIONS. ALL FEATURES SHOWN ARE APPROXIMATE INCLUDING INTERIOR PARTITIONS/WALLS, LOCATIONS OF SUB-SLAB PITS AND OTHER BUILDING FEATURES. CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL PROPOSED WORK.
- BUILDING FOUNDATION COMPONENTS WILL REMAIN IN PLACE AND BE PERFORATED FOR DRAINAGE UNLESS NOTED OTHERWISE.
- INSTALL A SIX-FOOT TALL TEMPORARY FENCE AROUND THE BUILDINGS DURING ABATEMENT AND DEMOLITION. REMOVE TEMPORARY FENCE AT COMPLETION.
- REFER TO CONTRACT SPECIFICATIONS FOR ASBESTOS AND OIL/HAZARDOUS MATERIAL ABATEMENT REQUIREMENTS.
- CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH THE TOWN TO OBTAIN ANY NECESSARY PERMITS/POLICE DETAILS. CONTRACTOR SHALL BEAR COSTS ASSOCIATED WITH ANY NECESSARY PERMITS/POLICE DETAILS.
- ALL EXISTING PAVED SURFACES AND CONCRETE PADS WITHIN THE LIMITS OF WORK AREA ON THE EXTERIOR PORTIONS OF THE BUILDING SCHEDULED TO BE DEMOLISHED SHALL REMAIN AND BE PROTECTED.
- ALL EXISTING CONCRETE OR PAVED SURFACES OUTSIDE WORK LIMITS SHALL BE PROTECTED AND REPAIRED IF DAMAGED FOLLOWING DEMOLITION.
- ALL PUBLIC ROADS MUST REMAIN OPEN TO TRAFFIC THROUGHOUT CONSTRUCTION.
- REMOVE ALL MISCELLANEOUS SITE DEBRIS LOCATED WITHIN WORK LIMITS.
- CERTAIN PAINTED BRICK AND CONCRETE MATERIALS ARE SCHEDULED FOR CRUSHING AND RE-USE.

UTILITY DEMOLITION NOTES (PERMIT LEVEL):

- TERMINATE, CUT AND CAP ALL UNDERGROUND UTILITIES SERVING THE BUILDINGS SCHEDULED TO BE DEMOLISHED (LOCATE VIA TEST PIT, IF NECESSARY), UNLESS OTHERWISE INDICATED.
- IF REQUIRED, COORDINATE WITH ELECTRIC COMPANY FOR TERMINATION OF ELECTRIC SERVICE. COORDINATE WITH PHONE COMPANY FOR TERMINATION OF PHONE SERVICE. PAY ALL ASSOCIATED FEES.
- IF REQUIRED, COORDINATE WITH GAS COMPANY FOR TERMINATION OF ALL BUILDING GAS SERVICES. DEMOLITION CONTRACTOR SHALL OBTAIN CLEARANCE FROM GAS COMPANY PRIOR TO BEGINNING ABATEMENT AND DEMOLITION WORK.
- INSTALL CONCRETE PLUGS IN GRAVITY LINES OUTSIDE THE FOUNDATION OF THE BUILDINGS. UTILITIES DESIGNATED TO REMAIN SHALL BE PROTECTED AND REPAIRED AT CONTRACTOR'S EXPENSE IF DAMAGED BY CONTRACTOR.
- IF REQUIRED, TERMINATE ALL WATER SERVICES TO THE BUILDINGS TO BE DEMOLISHED. COMPLETE TERMINATION IN ACCORDANCE WITH THE TOWN STANDARDS.
- IF REQUIRED, TERMINATE ALL SEWER AND DRAIN CONNECTIONS IN ACCORDANCE WITH TOWN REQUIREMENTS.

ACCESS RESTRICTIONS:

- NO VEHICLE HAVING MAXIMUM WHEEL LOADING IN EXCESS OF SIX THOUSAND (6,000) POUNDS WITH MIMIMUM AXLE SPACING OF AT LEAST FOUR AND ONE-HALF FEET (4.5') SHALL BE PERMITTED PASSAGE ALONG THE FIRSTLIGHT TRAVEL WAY; AND IF THE CANAL IS DEWATERED, (AS DETERMINED BY FIRSTLIGHT IN ITS SOLE DISCRETION), NO VEHICLE HAVING MAXIMUM WHEEL LOADING IN EXCESS OF FOUR THOUSAND (4,000) POUNDS SHALL BE PERMITTED PASSAGE ALONG THE FIRSTLIGHT TRAVEL WAY.

LEGEND

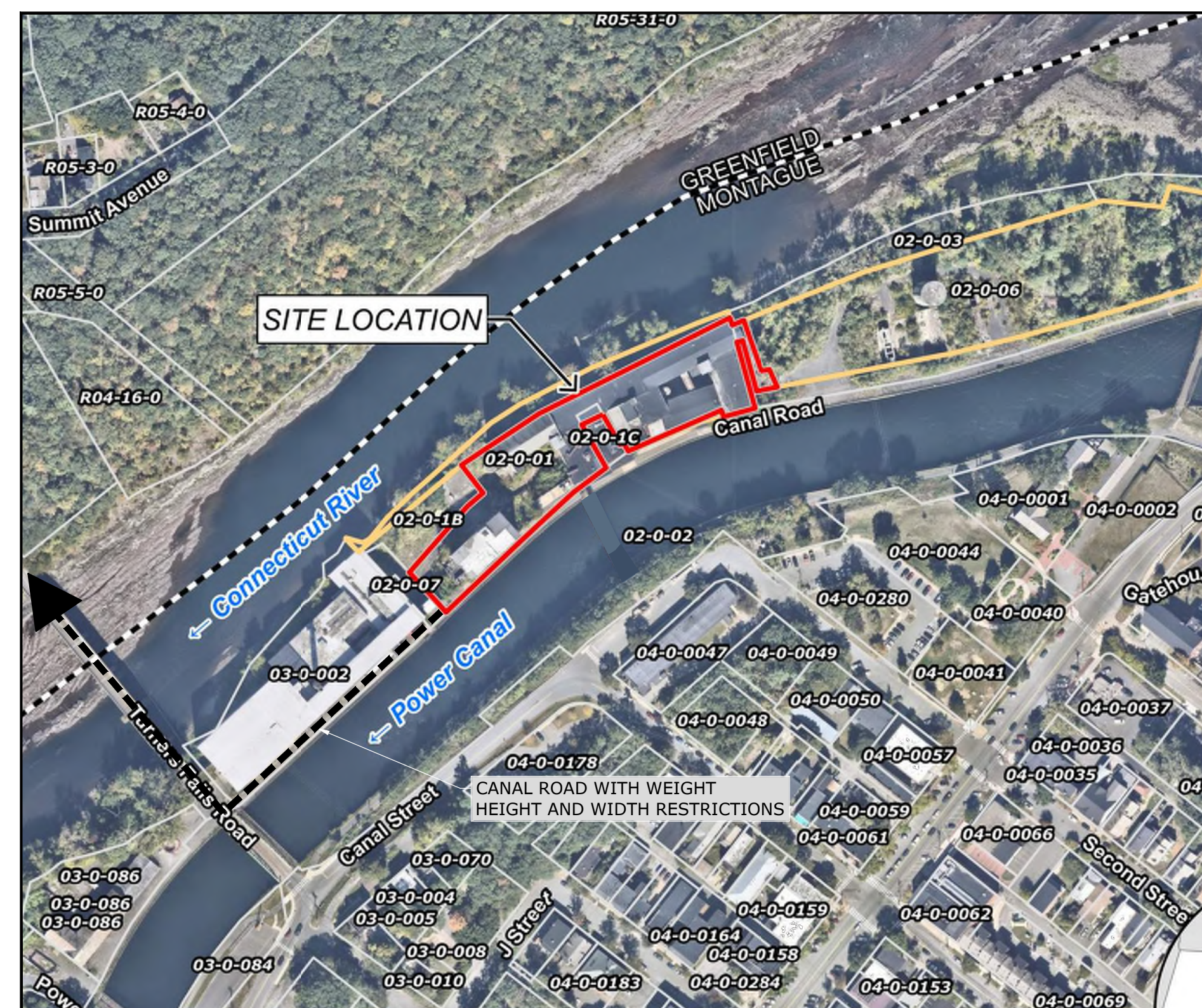
DESCRIPTION	EXISTING	PROPOSED
PROPERTY LINE	---	---
PROPERTY LINE ADJACENT	---	---
RIGHT-OF-WAY LINE	---	---
EASEMENT LINE	---	---
LIMITS OF WORK	---	---
INTERMEDIATE CONTOURS	---	---
INDEX CONTOURS	--- 25 ---	--- 25 ---
SPOT GRADE	X 141.2	+ 32.0
MAGNITUDE & DIRECTION OF SLOPE		← 0.0%
STORM DRAIN	SD SD	
GRAVITY SANITARY SEWER	SS SS	
WATER SERVICE	W W	
OVERHEAD ELECTRIC	OE OE	
CURB	====	
EDGE OF PAVEMENT	====	
FENCE - CHAIN LINK	---x---x---x---	
GUARDRAIL	---o---o---o---	
STORM DRAIN STRUCTURES	MANHOLE (D) CATCH BASIN (B)	
SANITARY SEWER STRUCTURES	MANHOLE (S)	
WATER SERVICE STRUCTURES	HYDRANT (H) MANHOLE (W) VALVE (V)	
TREELINE	~~~~~	

LEGEND

RESOURCE AREAS	
TOP OF BANK	---
100-FOOT BUFFER ZONE	---
200-FOOT RIVERFRONT AREA	---

LEGEND

DEMOLITION / GEOTECHNICAL	
EROSION & SEDIMENT CONTROL	-----



SITE LOCATION AND REQUIRED TRUCK ACCESS
SCALE: 1" = 200'

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FORMER STRATHMORE MILL COMPLEX ABATEMENT, DEMOLITION, AND RESTORATION PROJECT

Town of Montague

Montague, Massachusetts

MARK	DATE	DESCRIPTION
PROJECT NO:	M5003-012A	
DATE:	MAY 2026	
FILE:	M5003-012A-General Notes.dwg	
DRAWN BY:	A.LAPATIN	
DESIGNED BY:	D.BOULAIS	
CHECKED BY:	D.BOULAIS	
APPROVED BY:	T.KIRTON	

GENERAL NOTES & LEGEND

SCALE: NO SCALE

G-002

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FORMER STRATHMORE MILL COMPLEX ABATEMENT, DEMOLITION, AND RESTORATION PROJECT

Town of Montague

Montague, Massachusetts

MARK	DATE	DESCRIPTION
PROJECT NO:	M5003-012A	
DATE:	MAY 2026	
FILE:	M5003-012A-C-Existing Conditions.dwg	
DRAWN BY:	A.LAPATIN	
DESIGNED BY:	D.BOULAIS	
CHECKED BY:	D.BOULAIS	
APPROVED BY:	T.KIRTON	

EXISTING CONDITIONS & LIMITS OF WORK AREA PLAN

SCALE: 1" = 40'



LEGEND:

SITE BUILDING #1 - FIVE TOTAL STORIES/FLOORS, WITH BASE/1ST FLOOR SLAB ELEVATION APPROX. 149±

SITE BUILDING #2 - FIVE TOTAL STORIES/FLOORS, WITH BASE/1ST FLOOR SLAB ELEVATION APPROX. 149±

SITE BUILDING #3 - FIVE TOTAL STORIES/FLOORS, WITH BASE/1ST FLOOR SLAB ELEVATION APPROX. 149±

SITE BUILDING #4 - FOUR TOTAL STORIES/FLOORS, WITH BASE/1ST FLOOR SLAB ELEVATION APPROX. 149±

SITE BUILDING #5/5A - FOUR TOTAL STORIES/FLOORS, WITH BASE/1ST FLOOR SLAB ELEVATION APPROX. 149±

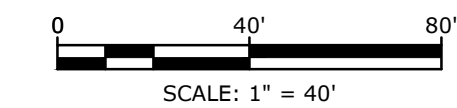
SITE BUILDING #6/6A - FOUR TO FIVE TOTAL STORIES/FLOORS, WITH BASE/1ST FLOOR SLAB ELEVATION APPROX. 149±

SITE BUILDING #7 - FOUR TOTAL STORIES/FLOORS, WITH BASE/1ST FLOOR SLAB ELEVATION APPROX. 146± AND OPEN VOID BENEATH SLAB (MOSTLY EARTHEN BASE) W/ BOTTOM ELEVATION 142±

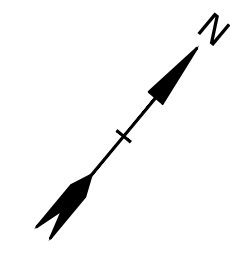
SITE BUILDING #8 - TWO TOTAL STORIES/FLOORS (2ND AND 3RD FLOORS OF BUILDING COMPLEX), WITH OPEN VOID BENEATH 2ND FLOOR (MOSTLY EARTHEN BASE) W/BOTTOM ELEVATION 142±

SITE BUILDING #11 - SEVEN TOTAL STORIES/FLOORS, WITH BASE/1ST FLOOR SLAB ELEVATION APPROX. 149± (WESTERN END ADDITION OF BUILDING IS OPEN ROOM, 2 TO 3 STORIES IN HEIGHT)

(OFF-SITE BUILDING #9 - FOUR TOTAL STORIES/FLOORS, WITH BASE/1ST FLOOR SLAB ELEVATION APPROX. 149±)



Last Saved: 5/20/2026 5:45pm By: A.Lapatin
 Plotted On: Jun 08, 2026 6:45pm By: A.Lapatin
 Tighe & Bond 53 Southampton Road Westfield, MA 01085
 Design Phase Drawings AutoCAD Sheet Drawings M5003-012A-C-Existing Conditions.dwg



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FORMER STRATHMORE MILL COMPLEX ABATEMENT, DEMOLITION, AND RESTORATION PROJECT

Town of Montague

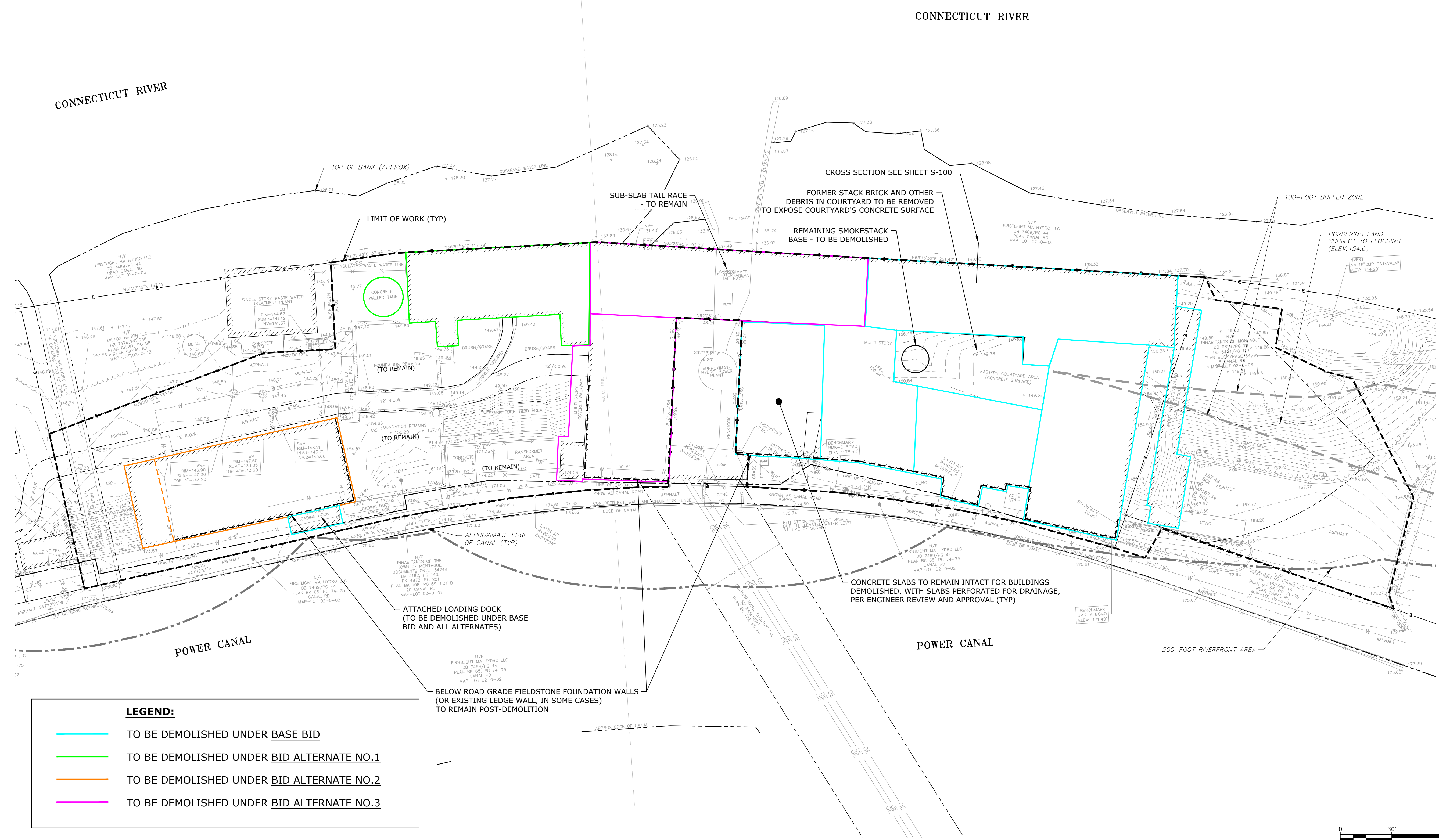
Montague, Massachusetts

MARK	DATE	DESCRIPTION
PROJECT NO:	M5003-012A	
DATE:	MAY 2026	
FILE:	M5003-012A-C-Demo.dwg	
DRAWN BY:	A.LAPATIN	
DESIGNED BY:	D.BOULAIS	
CHECKED BY:	D.BOULAIS	
APPROVED BY:	T.KIRTON	

DEMOLITION PLAN

SCALE: 1" = 30'

C-200

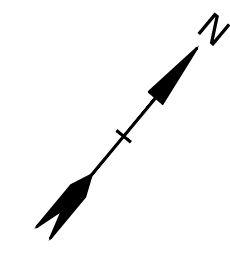


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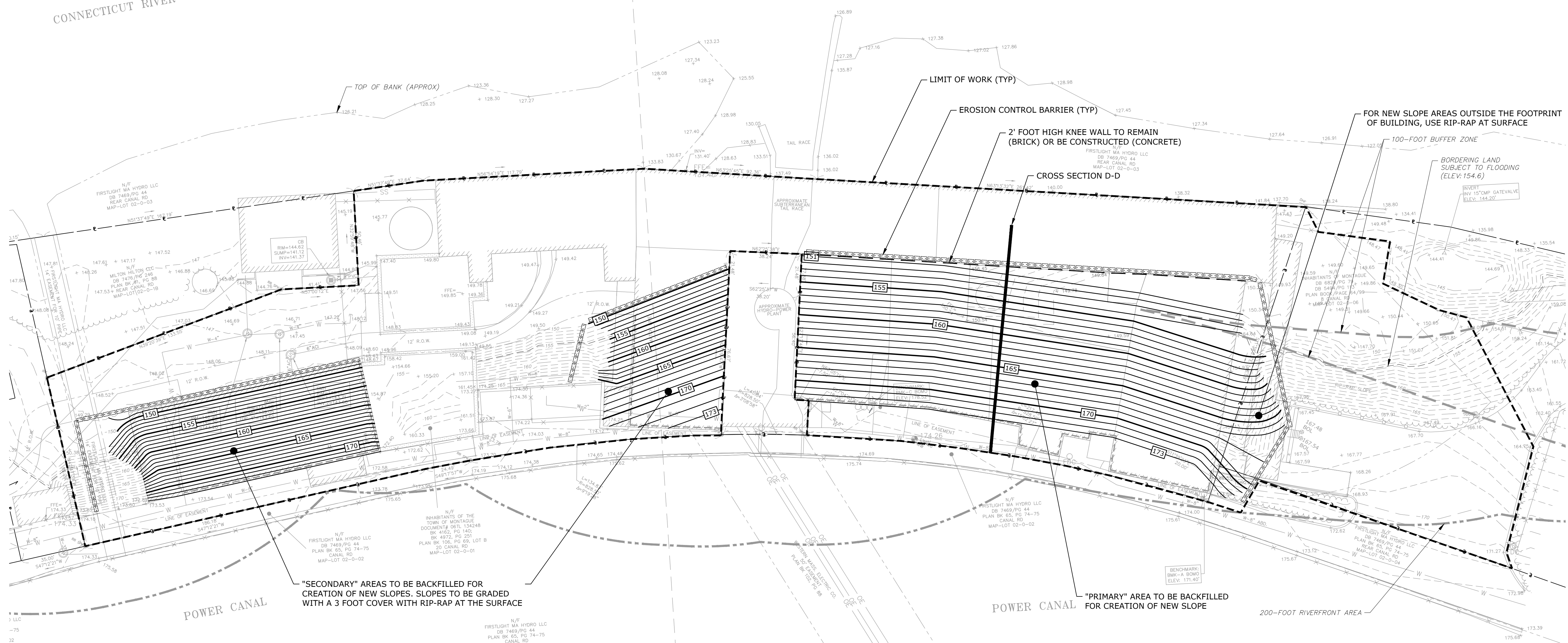
- TO BE DEMOLISHED UNDER BASE BID
- TO BE DEMOLISHED UNDER BID ALTERNATE NO.1
- TO BE DEMOLISHED UNDER BID ALTERNATE NO.2
- TO BE DEMOLISHED UNDER BID ALTERNATE NO.3



Last Saved: 5/20/2026 5:20:06 PM
 Plotted On: Jun 08, 2026 6:45pm By: Alapatin
 Tighe & Bond\3\155003\Montague\012A-Strathmore-Demo-Design-Phase\Drawings\AutoCAD\Sheet\Permit-Drawings\M5003-012A-C-Demo.dwg



CONNECTICUT RIVER



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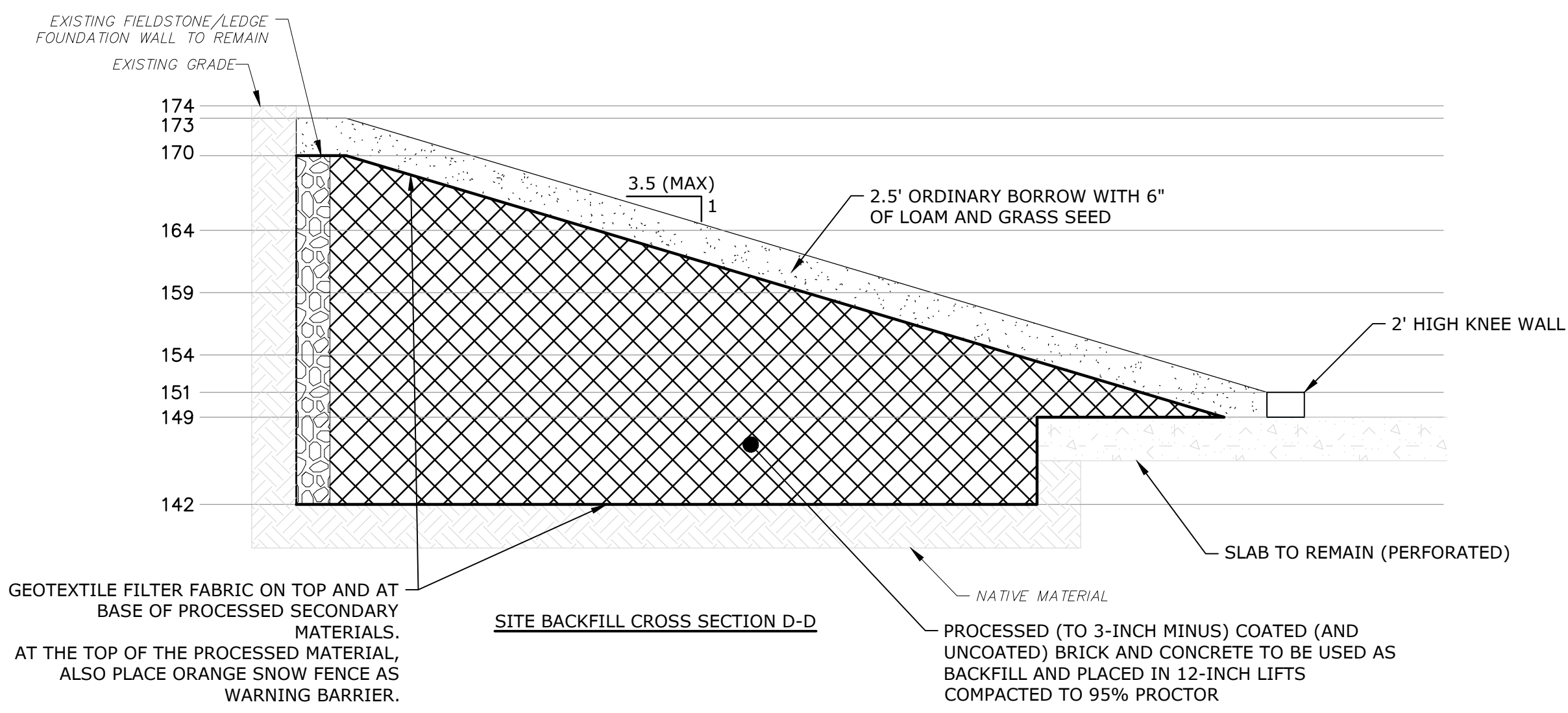
FORMER STRATHMORE MILL COMPLEX ABATEMENT, DEMOLITION, AND RESTORATION PROJECT

Town of Montague

Montague, Massachusetts

"SECONDARY" AREAS TO BE BACKFILLED FOR CREATION OF NEW SLOPES. SLOPES TO BE GRADED WITH A 3 FOOT COVER WITH RIP-RAP AT THE SURFACE

"PRIMARY" AREA TO BE BACKFILLED FOR CREATION OF NEW SLOPE



GEOTEXTILE FILTER FABRIC ON TOP AND AT BASE OF PROCESSED SECONDARY MATERIALS. AT THE TOP OF THE PROCESSED MATERIAL, ALSO PLACE ORANGE SNOW FENCE AS WARNING BARRIER.

PROCESSED (TO 3-INCH MINUS) COATED (AND UNCOATED) BRICK AND CONCRETE TO BE USED AS BACKFILL AND PLACED IN 12-INCH LIFTS COMPACTED TO 95% PROCTOR



MARK	DATE	DESCRIPTION
PROJECT NO:	M5003-012A	
DATE:	MAY 2026	
FILE:	M5003-012A-C-Restoration.dwg	
DRAWN BY:	A.LAPATIN	
DESIGNED BY:	D.BOULAIS	
CHECKED BY:	D.BOULAIS	
APPROVED BY:	T.KIRTON	

SITE RESTORATION & GRADING PLAN

SCALE: 1" = 30'

C-300

Last Saved: 5/20/2026 4:45pm By: ALAPATIN
Printed On: Jun 08, 2026 6:45pm By: ALAPATIN
Tighe & Bond\3\155003_Montague\012A_Strathmore_Demo_Design_Phase\Drawings\AutoCAD\Sheet\Permit Drawings\M5003-012A-C-Restoration.dwg

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FORMER STRATHMORE MILL COMPLEX ABATEMENT, DEMOLITION, AND RESTORATION PROJECT

Town of Montague

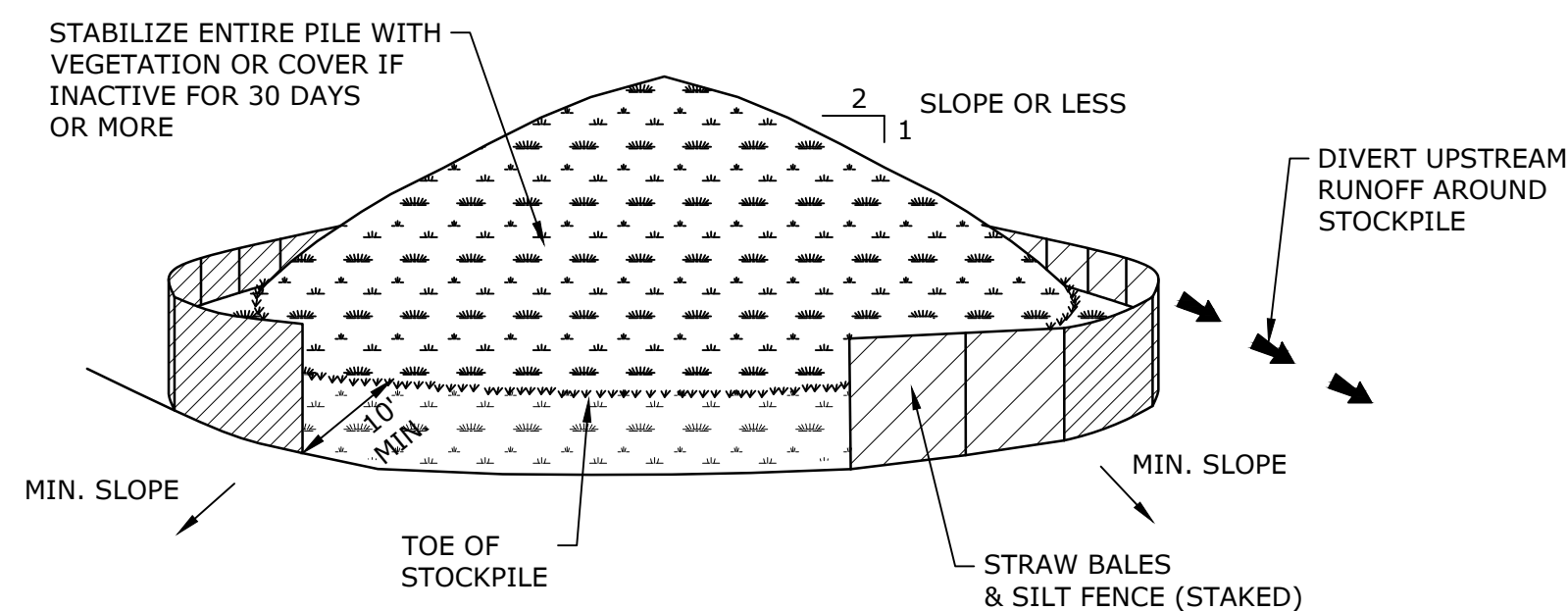
Montague, Massachusetts

MARK	DATE	DESCRIPTION
PROJECT NO: M5003-012A		
DATE: MAY 2026		
FILE: M5003-012A-Details.dwg		
DRAWN BY: A.LAPATIN		
DESIGNED BY: D.BOULAIS		
CHECKED BY: D.BOULAIS		
APPROVED BY: T.KIRTON		

SITE DETAILS

SCALE: NO SCALE

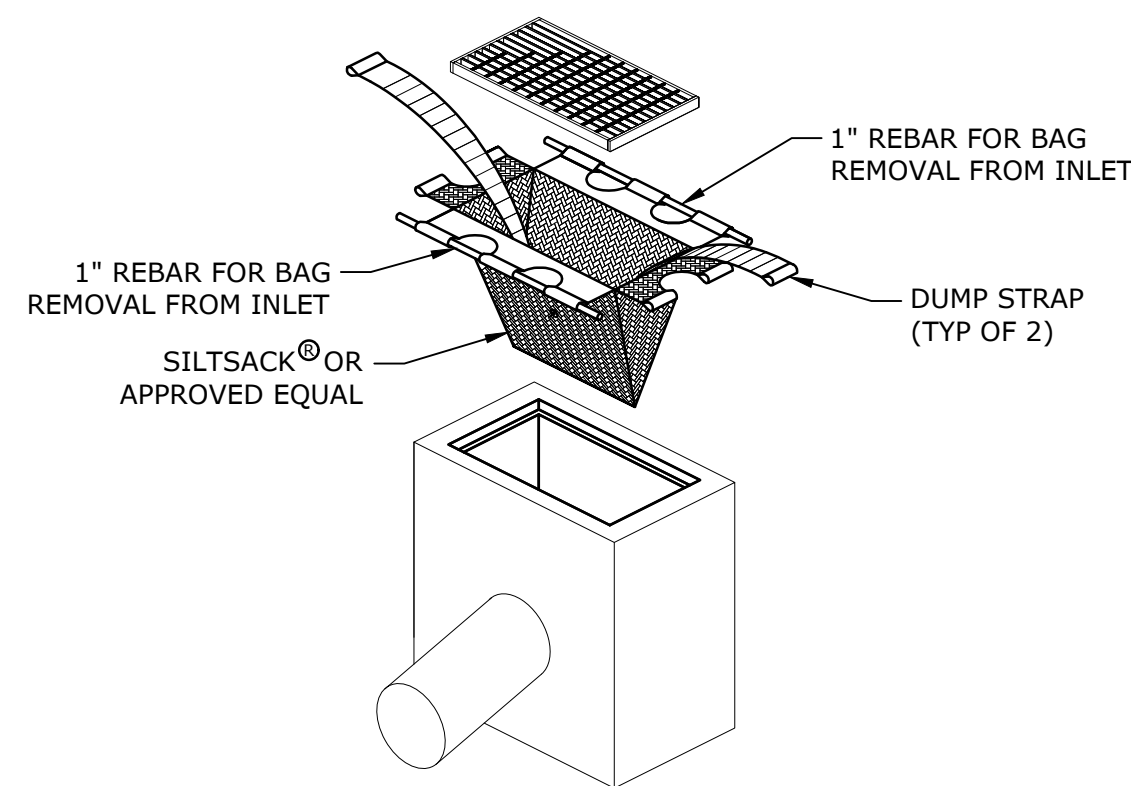
C-500



INSTALLATION NOTES:

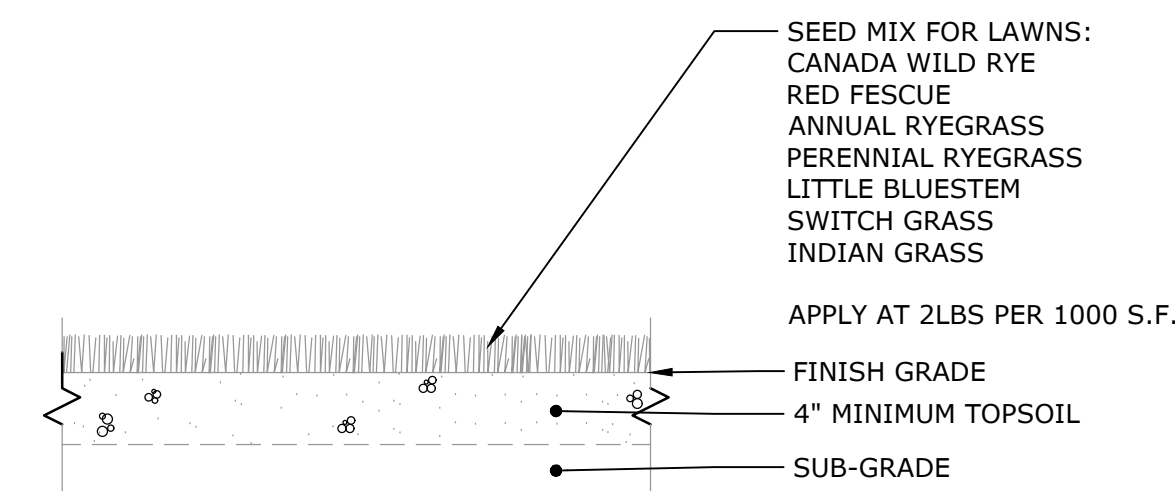
1. AREA CHOSEN FOR STOCKPILING OPERATIONS SHALL BE DRY AND STABLE.
2. MAXIMUM SLOPE OF STOCKPILE SHALL BE 2H:1V.
3. UPON COMPLETION OF SOIL STOCKPILING, EACH PILE SHALL BE SURROUNDED WITH EITHER SILT FENCING OR STRAW BALES, THEN STABILIZED WITH VEGETATION OR COVERED.

SOIL STOCKPILING
NO SCALE

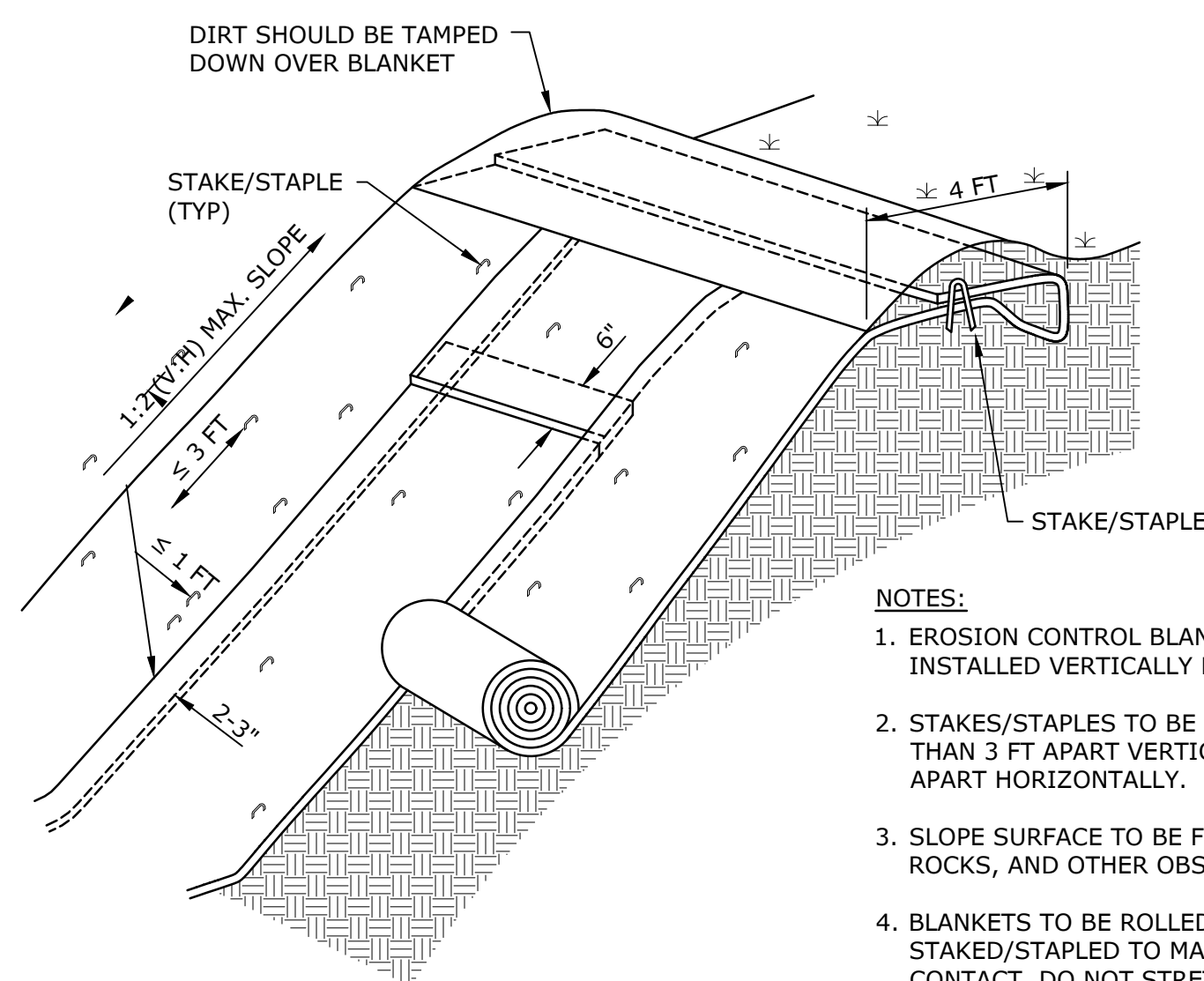


SILTSACK MANUFACTURED BY:
ACF ENVIRONMENTAL
2831 CARDWELL ROAD
RICHMOND, VIRGINIA 23237

SILTSACK® OR APPROVED EQUAL
NO SCALE



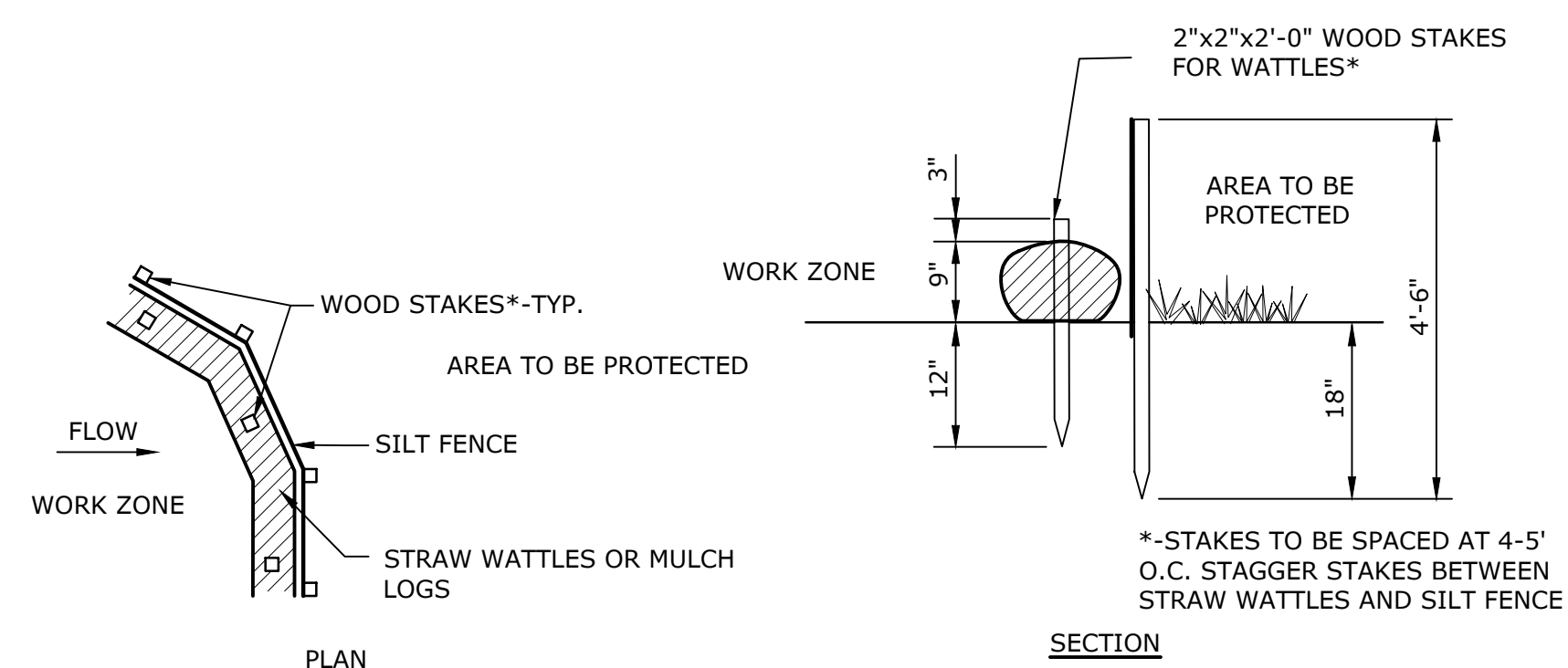
LAWN AREAS
NO SCALE



NOTES:

1. EROSION CONTROL BLANKET TO BE INSTALLED VERTICALLY DOWNSLOPE.
2. STAKES/STAPLES TO BE PLACED NO MORE THAN 3 FT APART VERTICALLY, AND 1 FT APART HORIZONTALLY.
3. SLOPE SURFACE TO BE FREE OF STICKS, ROCKS, AND OTHER OBSTRUCTIONS.
4. BLANKETS TO BE ROLLED OUT LOOSELY AND STAKED/STAPLED TO MAINTAIN DIRECT SOIL CONTACT. DO NOT STRETCH THE BLANKETS.
5. AREA TO BE SEEDED WITH SLOPE RESTORATION SEED MIX.

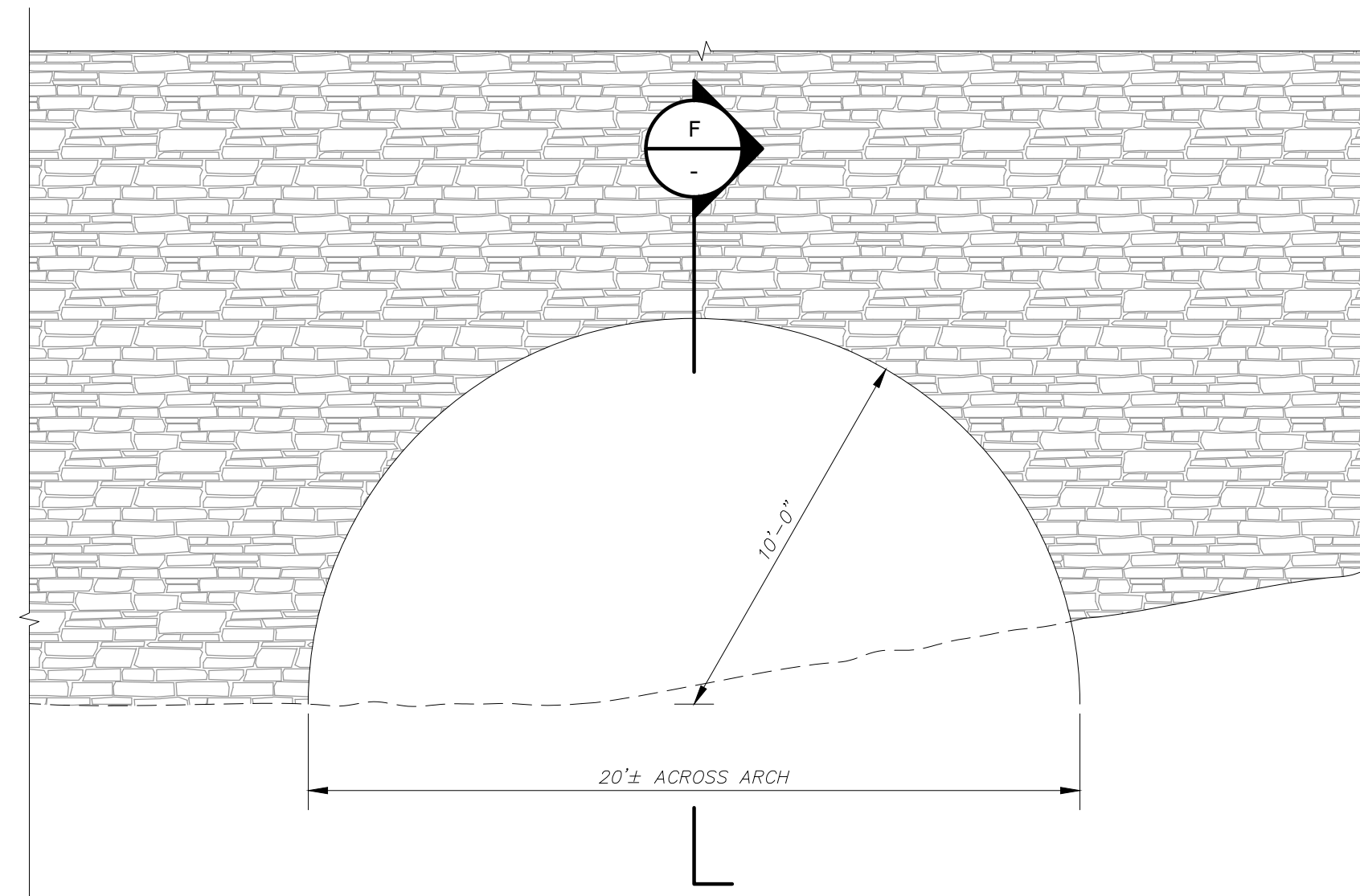
EROSION CONTROL BLANKET
NO SCALE



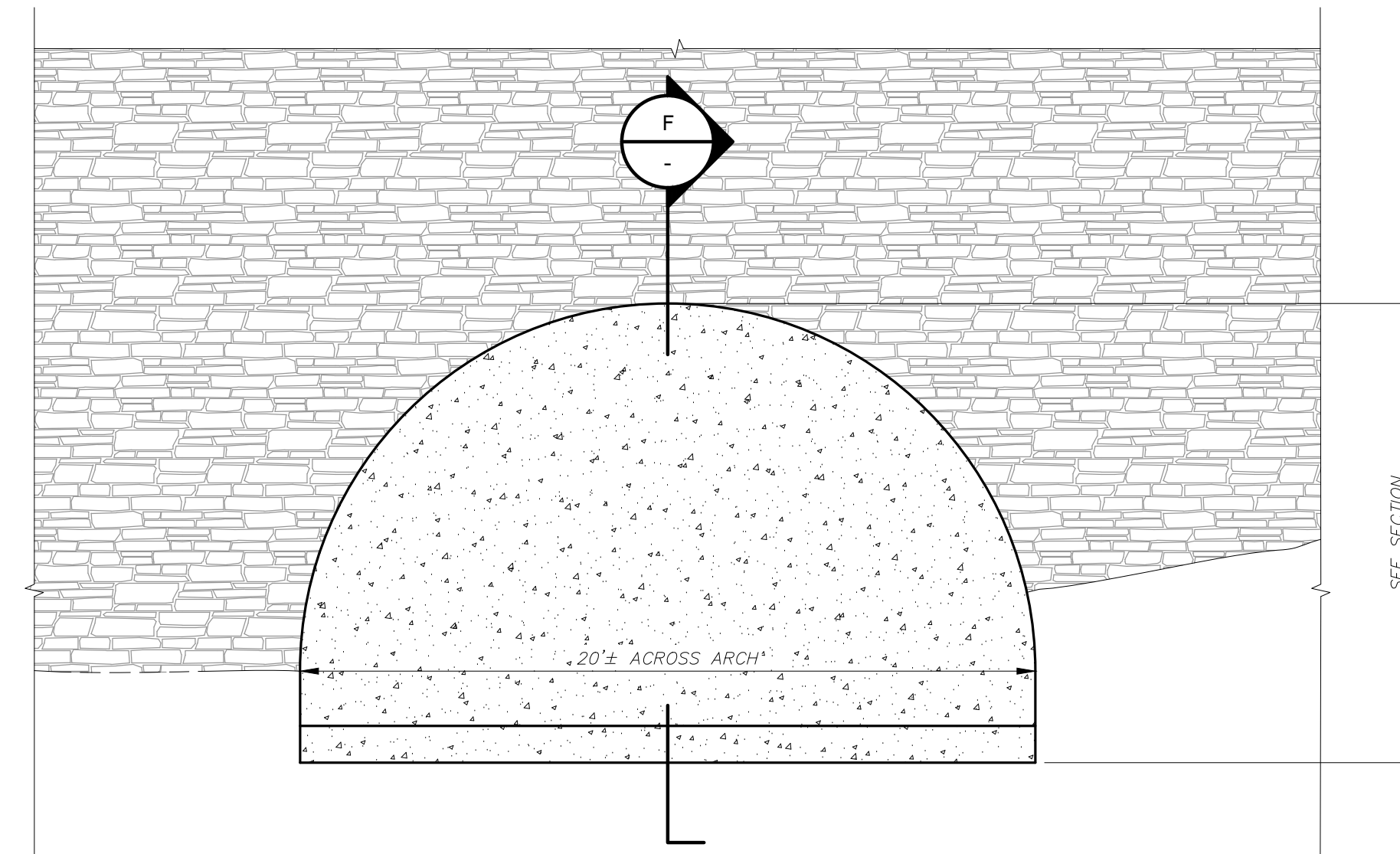
EROSION CONTROL BARRIER
NO SCALE



PHOTO #1



EXISTING TAILRACE #3

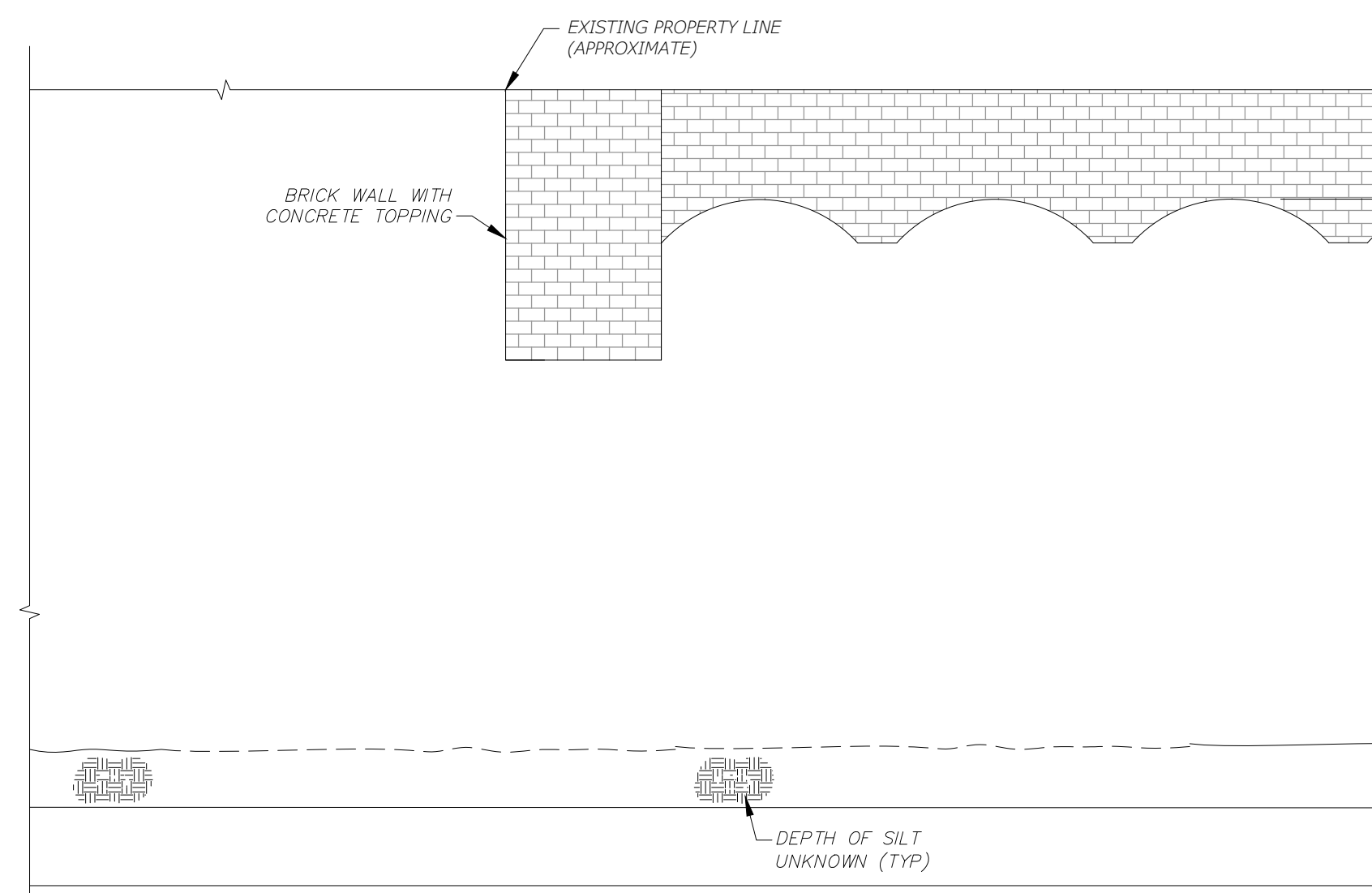


PROPOSED TAILRACE CAP

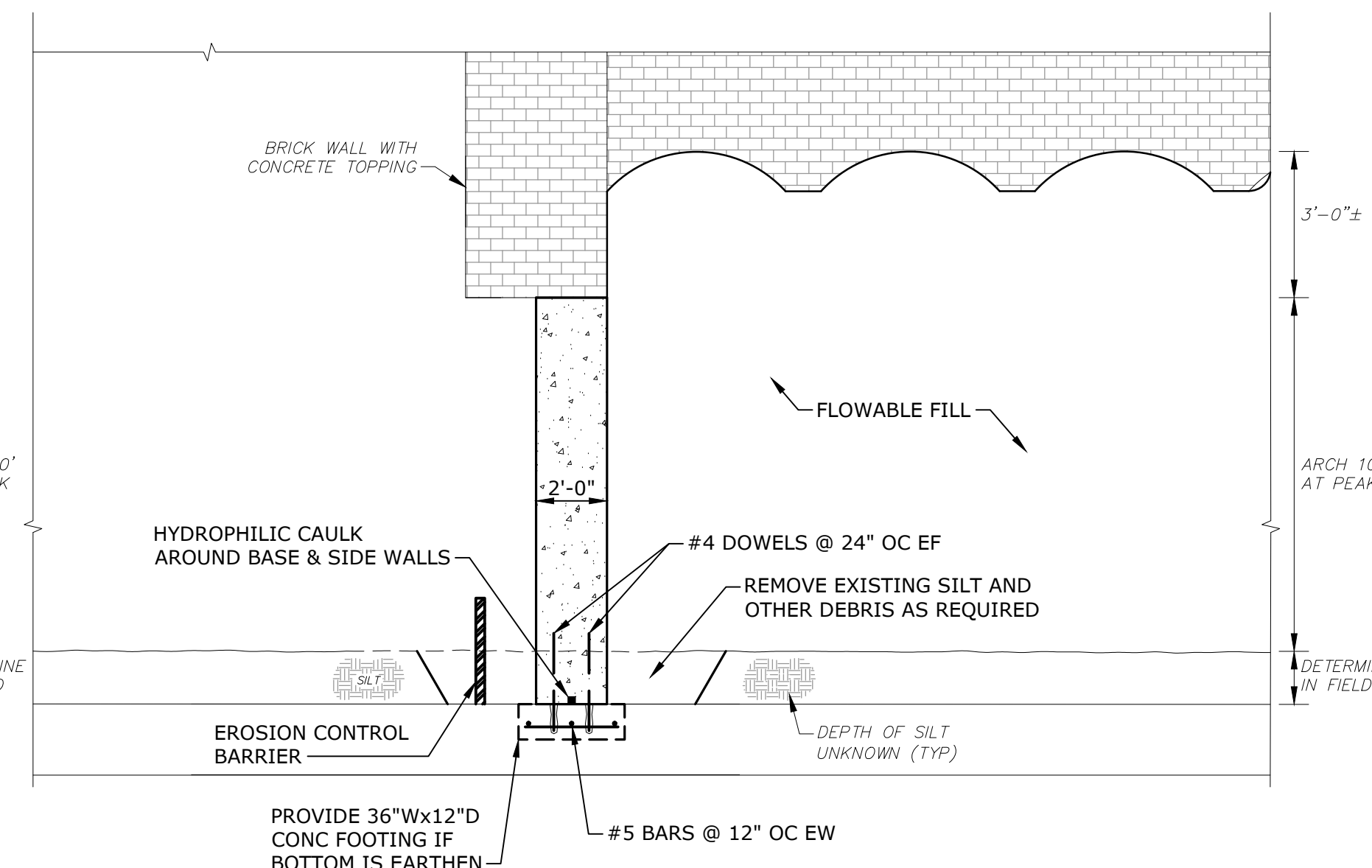
SECTION E
1/4" = 1'-0"



PHOTO #2



EXISTING TAILRACE SECTION



PROPOSED TAILRACE CAP SECTION

SECTION F
1/4" = 1'-0"

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FORMER STRATHMORE MILL COMPLEX ABATEMENT, DEMOLITION, AND RESTORATION PROJECT

Town of Montague

Montague, Massachusetts

MARK	DATE	DESCRIPTION
PROJECT NO:	M5003-012A	
DATE:	MAY 2026	
FILE:	M5003-012A-S-100.dwg	
DRAWN BY:	A.LAPATIN	
DESIGNED BY:	D.BOULAIS	
CHECKED BY:	D.BOULAIS	
APPROVED BY:	T.KIRTON	

TAILRACE #3 CLOSURE DETAILS

SCALE: NO SCALE

S-100

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FORMER STRATHMORE MILL COMPLEX ABATEMENT, DEMOLITION, AND RESTORATION PROJECT

Town of Montague

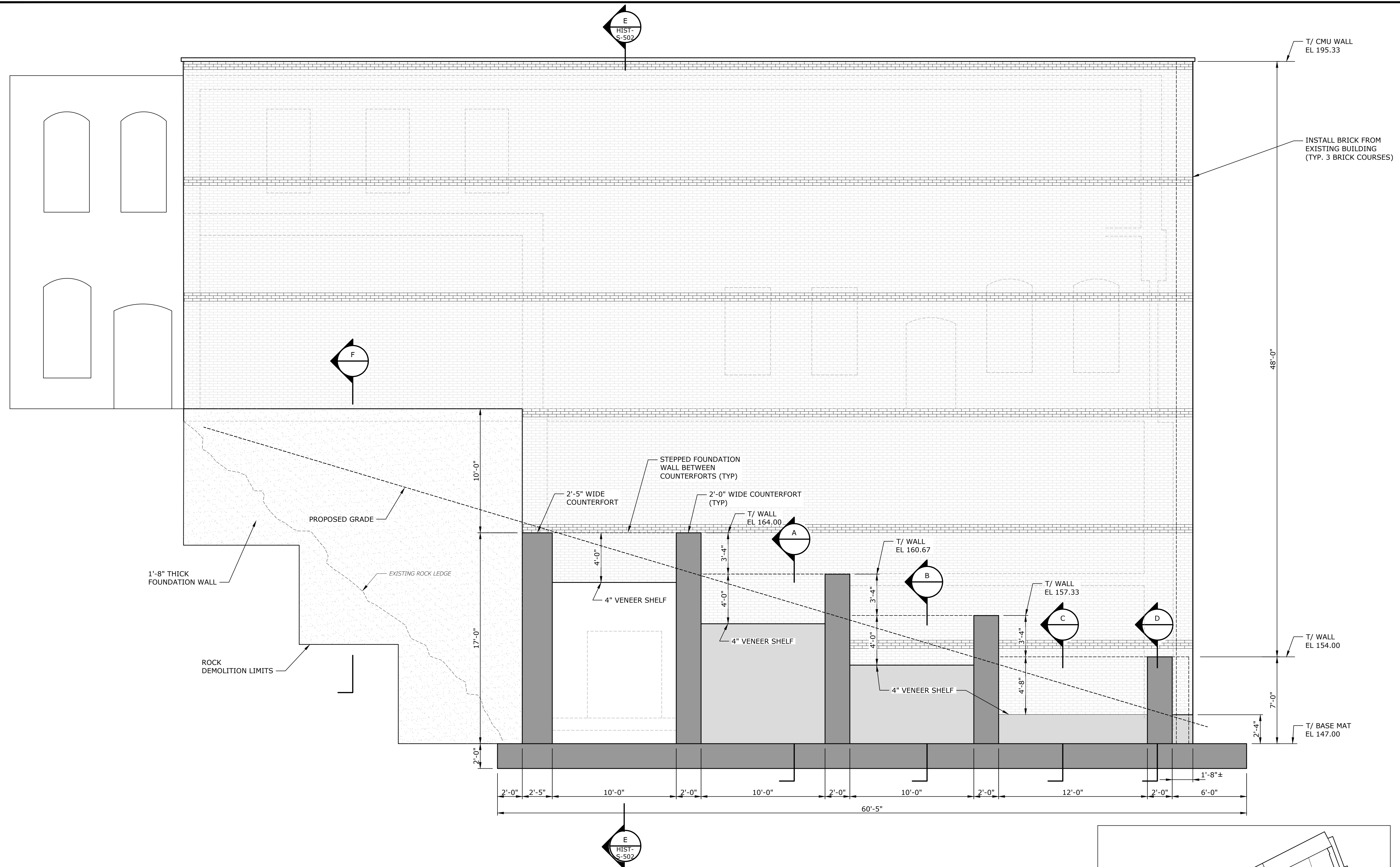
Montague, Massachusetts

MARK	DATE	DESCRIPTION
PROJECT NO:	M5003-012A	
DATE:	MAY 2026	
FILE:	M5003-012A-RetWall.dwg	
DRAWN BY:	D.BISHOP	
DESIGNED BY:	R.PEASE	
CHECKED BY:	M.K.HEDERIAN	
APPROVED BY:	C.FRENCH	

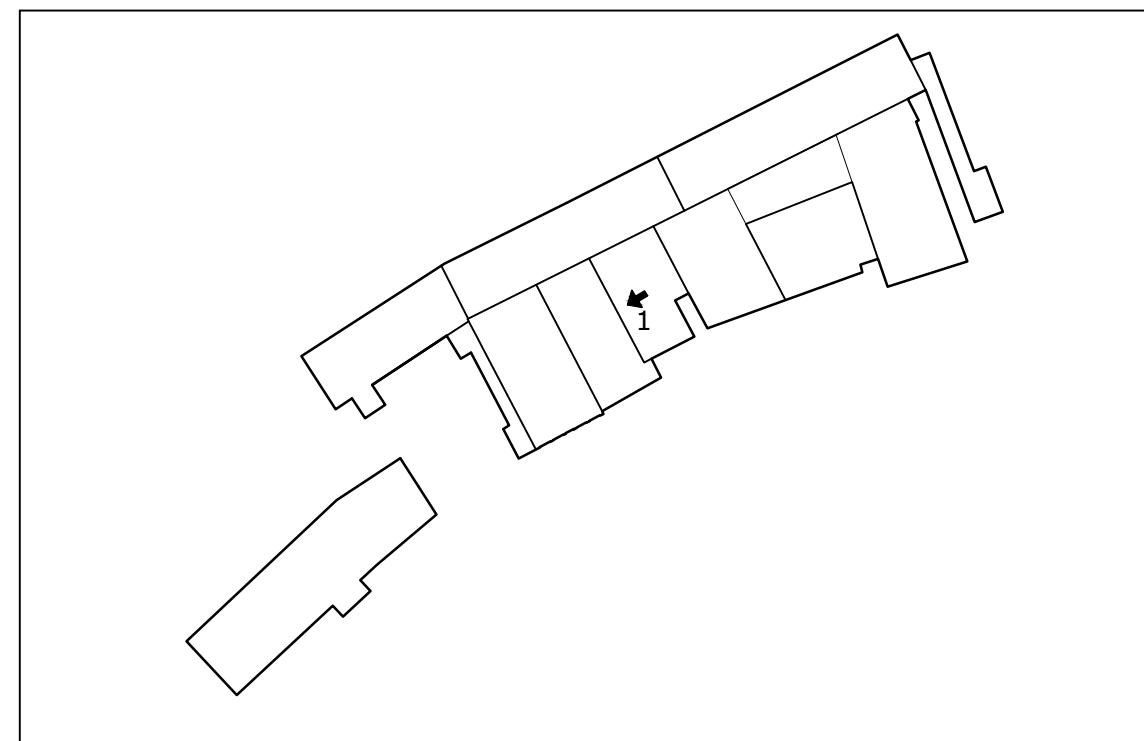
STRUCTURAL WALL ELEVATIONS 1 - HISTORICAL

SCALE: AS SHOWN

HIST-S-301



ELEVATION 1
RETAINING WALL/END WALL SUPPORT FOUNDATION
1/4" = 1'-0"



KEY PLAN
NO SCALE

Last Saved: 6/8/2026 12:55pm By: SP/rd
 Plotted On: Jun 10, 2026 12:55pm
 Tighe & Bond 33 W. Pleasant St. Montague, MA 01462

ISSUED FOR PERMIT - NOT FOR CONSTRUCTION

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FORMER STRATHMORE MILL COMPLEX ABATEMENT, DEMOLITION, AND RESTORATION PROJECT

Town of Montague

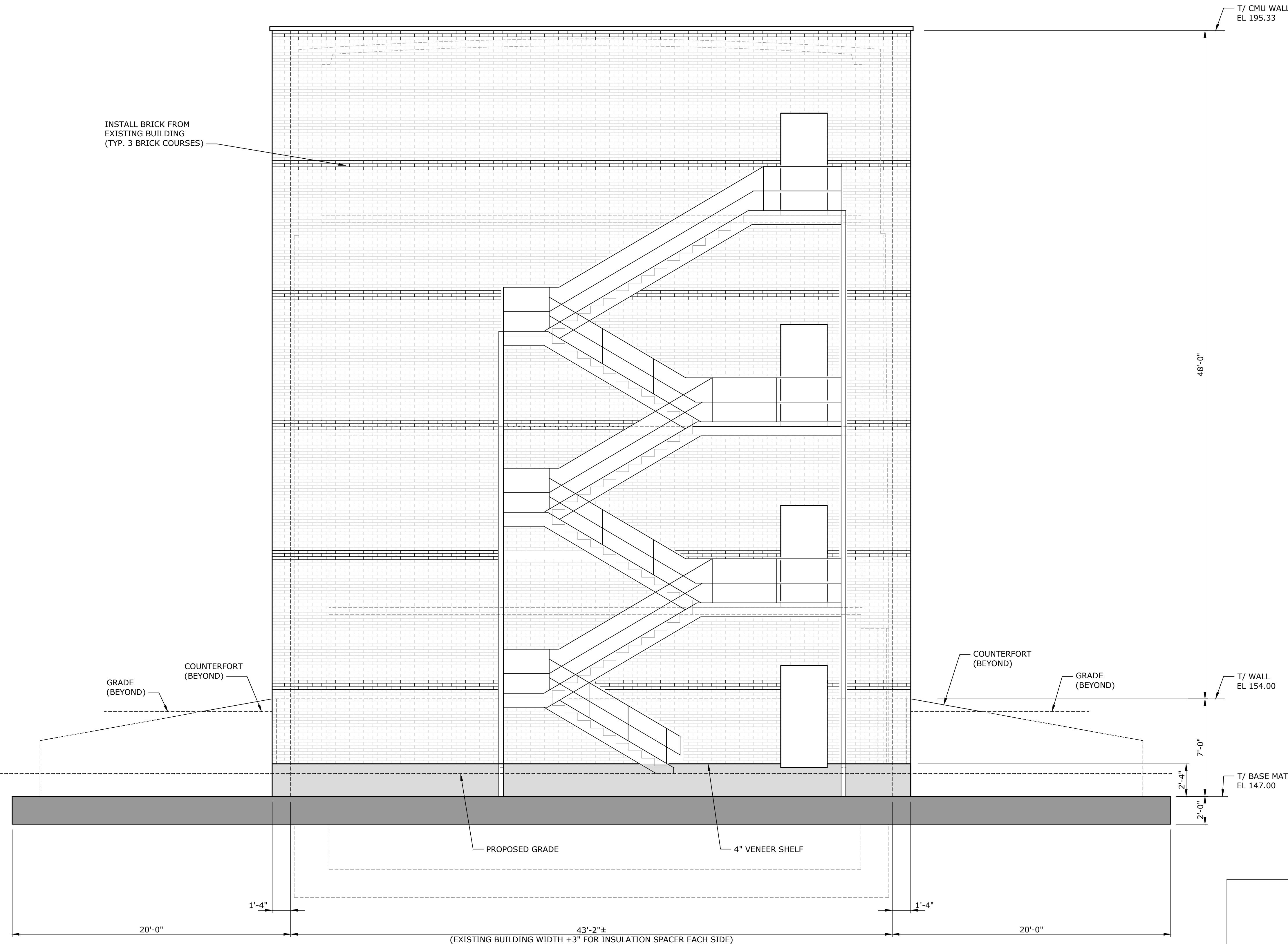
Montague, Massachusetts

MARK	DATE	DESCRIPTION
PROJECT NO:	M5003-012A	
DATE:	MAY 2026	
FILE:	M5003-012A-RetWall.dwg	
DRAWN BY:	D.BISHOP	
DESIGNED BY:	R.PEASE	
CHECKED BY:	M.KHEDERIAN	
APPROVED BY:	C.FRENCH	

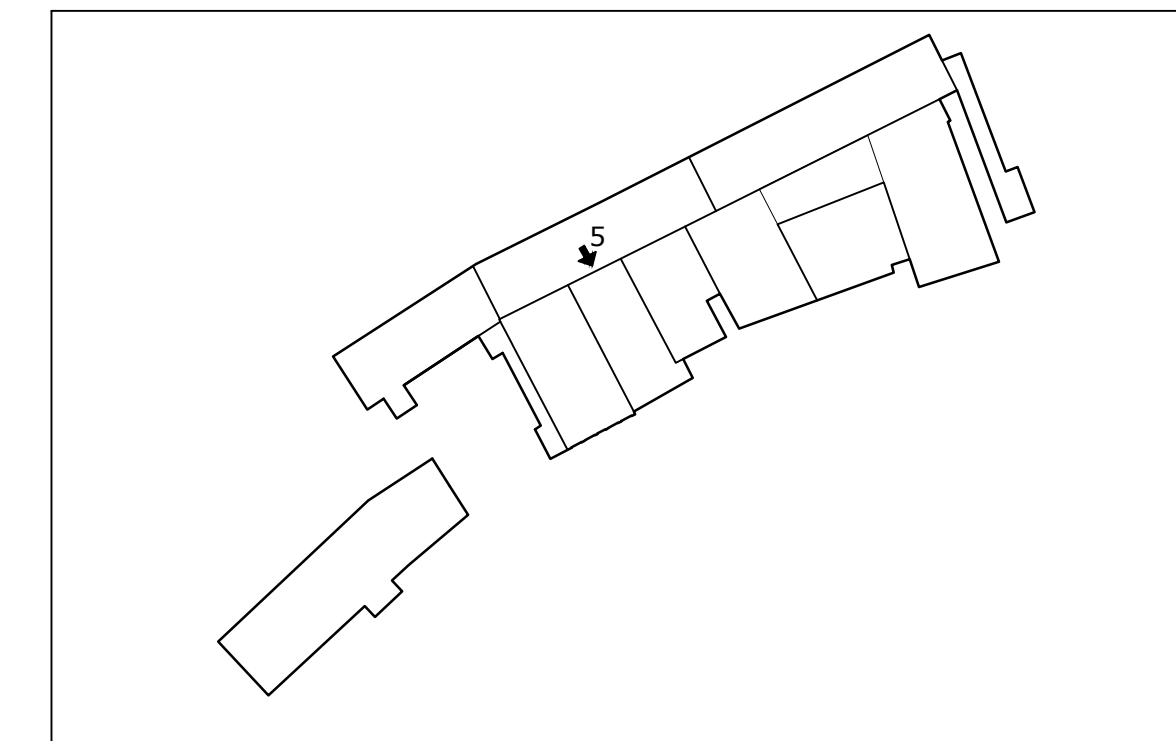
STRUCTURAL WALL ELEVATIONS 2 - HISTORICAL

SCALE: AS SHOWN

HIST-S-302



ELEVATION 5
RETAINING WALL/END WALL SUPPORT FOUNDATION
1/4" = 1'-0"



KEY PLAN
NO SCALE

INSTALL BRICK FROM EXISTING BUILDING (TYP. 3 BRICK COURSES)

GRADE (BEYOND)
COUNTERFORT (BEYOND)

COUNTERFORT (BEYOND)
GRADE (BEYOND)

PROPOSED GRADE
43'-2"±
(EXISTING BUILDING WIDTH + 3" FOR INSULATION SPACER EACH SIDE)

4" VENEER SHELF

Last Saved: 6/4/2026 11:16pm By: SP946
 Plotted On: Jun 04, 2026 4:16pm
 Tighe & Bond 53 Southampton Road Westfield, MA 01085
 Project: Phase 1 Drawings AutoCAD Sheet M5003-012A-RetWall.dwg

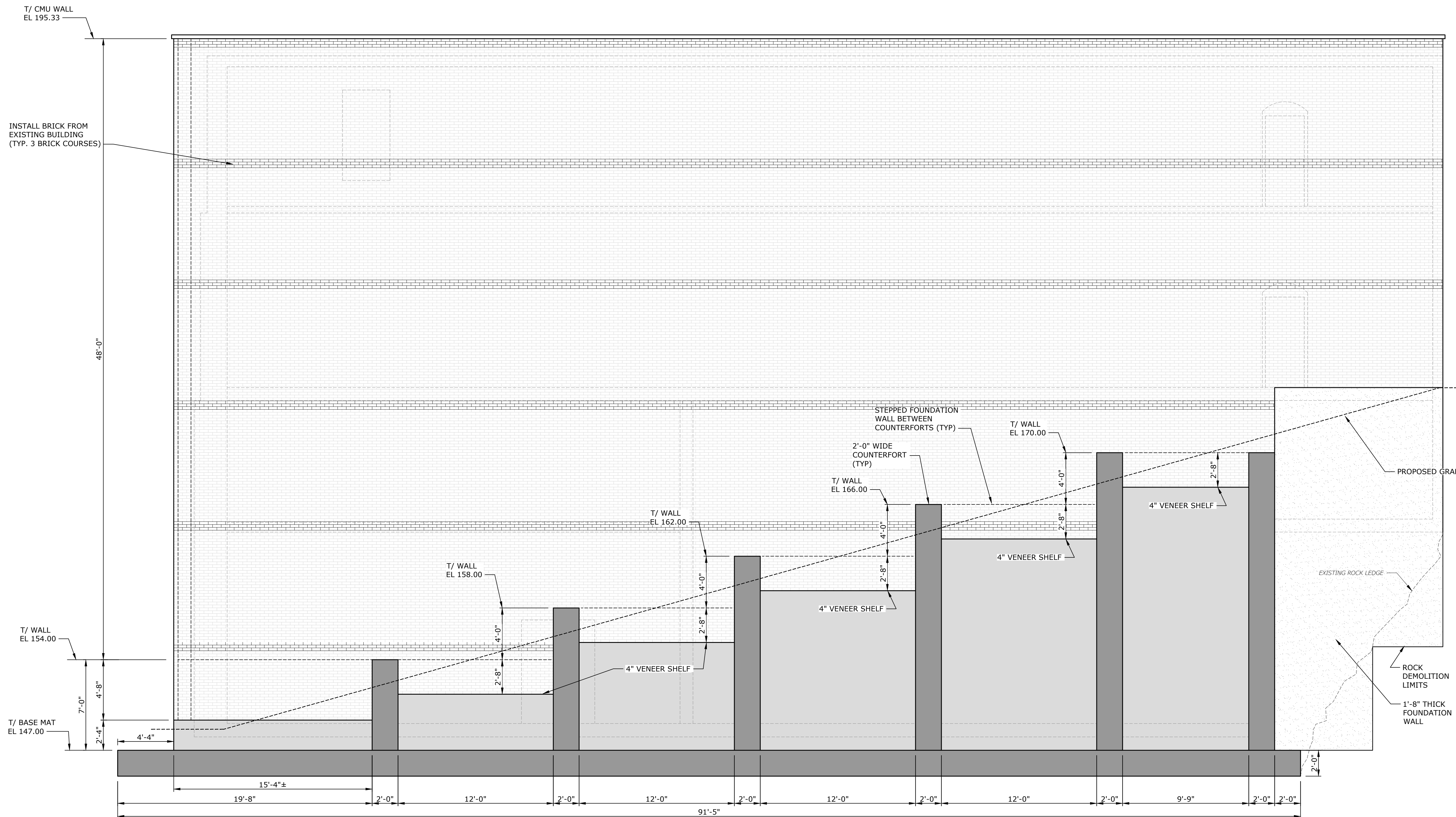
ISSUED FOR PERMIT - NOT FOR CONSTRUCTION

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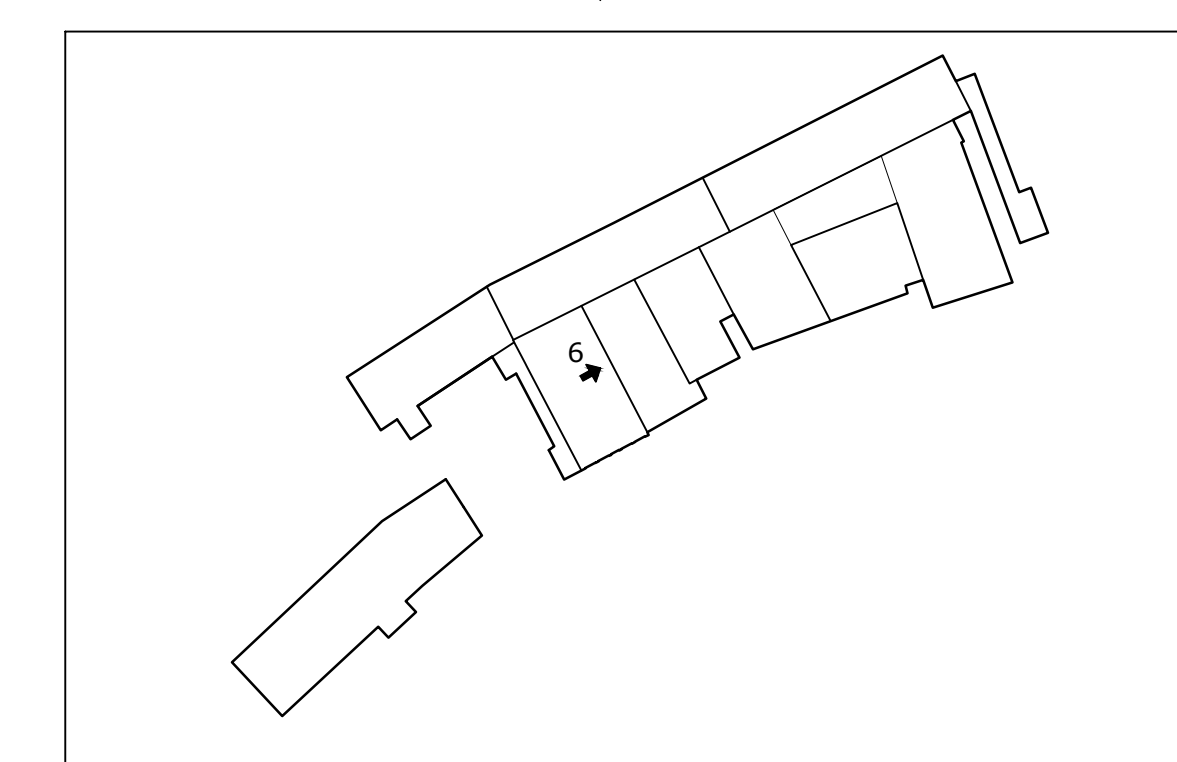
FORMER STRATHMORE MILL COMPLEX ABATEMENT, DEMOLITION, AND RESTORATION PROJECT

Town of Montague

Montague, Massachusetts



**ELEVATION 6
RETAINING WALL/END WALL SUPPORT FOUNDATION**
1/4" = 1'-0"



KEY PLAN
NO SCALE

MARK	DATE	DESCRIPTION
PROJECT NO:	M5003-012A	
DATE:	MAY 2026	
FILE:	M5003-012A-RetWall.dwg	
DRAWN BY:	D.BISHOP	
DESIGNED BY:	R.PEASE	
CHECKED BY:	M.KHEDERIAN	
APPROVED BY:	C.FRENCH	

STRUCTURAL WALL
ELEVATIONS 3 -
HISTORICAL

SCALE: AS SHOWN

HIST-S-303

Last Saved: 6/4/2026 11:14pm By: S9946
 Plotted On: Jun 04, 2026 11:14pm
 Tighe & Bond 53 Southampton Road Westfield, MA 01085
 Project: Former Strathmore Mill Complex Abatement, Demolition, and Restoration Project
 Design Phase: Drawings/ArchCAD/Sheet/M5003-012A-RetWall.dwg

ISSUED FOR PERMIT - NOT FOR CONSTRUCTION

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FORMER STRATHMORE MILL COMPLEX ABATEMENT, DEMOLITION, AND RESTORATION PROJECT

Town of Montague

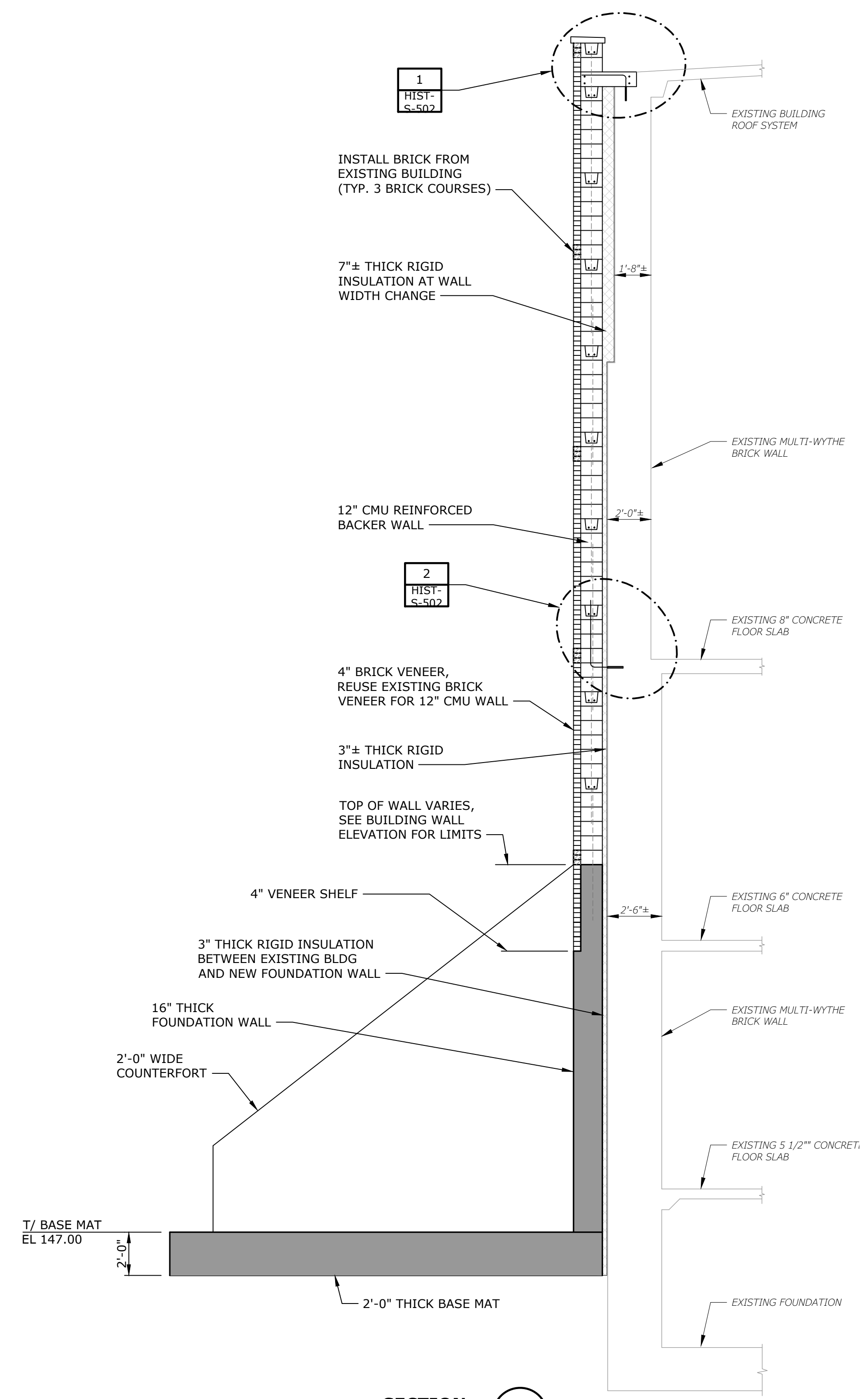
Montague, Massachusetts

MARK	DATE	DESCRIPTION
PROJECT NO:	M5003-012A	
DATE:	MAY 2026	
FILE:	M5003-012A-RetWall.dwg	
DRAWN BY:	D.BISHOP	
DESIGNED BY:	R.PEASE	
CHECKED BY:	M.KHEDERIAN	
APPROVED BY:	C.FRENCH	

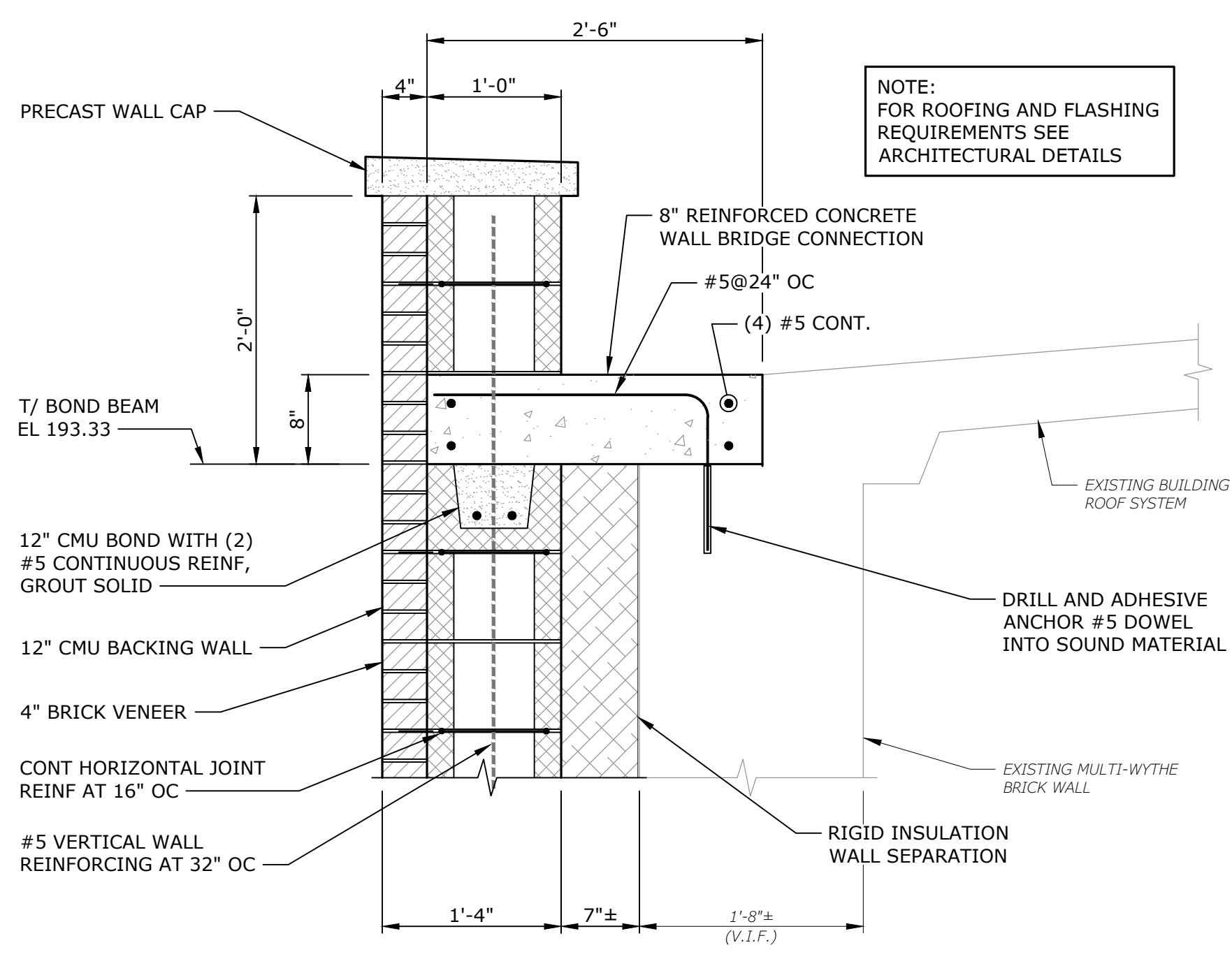
STRUCTURAL DETAILS - HISTORICAL

SCALE: AS SHOWN

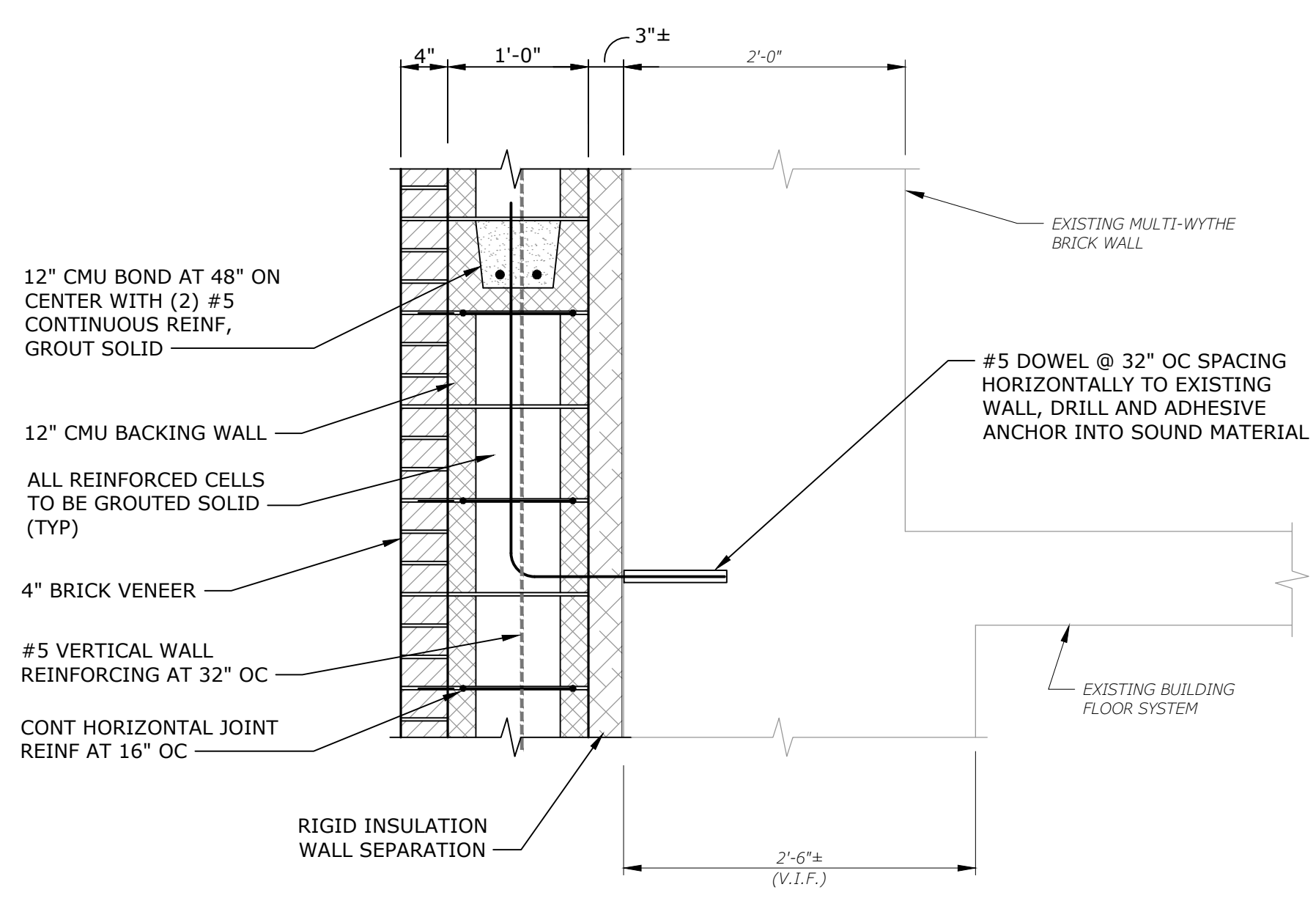
HIST-S-502



SECTION E
1/4" = 1'-0"
HIST-S-301



DETAIL 1
1" = 1'-0"
HIST-S-502



DETAIL 2
1" = 1'-0"
HIST-S-502

Last Saved: 6/4/2026 9:26pm By: SP9rd
 Plotted On: Jun 04, 2026 4:26pm
 Tighe & Bond 53 Southampton Road Westfield, MA 01085
 Phase: Drawings/ArchCAD/Sheet/M5003-012A-RetWall.dwg


Project: Strathmore Mill Demolition Project


Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex

Location: Montague, MA

Photograph No.: 1	Date: 1/23/2026	Direction Taken: South
Description: View of Building #1 from the Connecticut River. The fieldstone foundation walls (outlined in red) for Buildings #1, #2, and #3 to the river bank level will remain.		
		

Photograph No.: 2	Date: 1/23/2026	Direction Taken: East
Description: View of exterior of Building #4 from Courtyard West. The fieldstone foundation (outlined in red) displayed here will remain.		
		

Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

Photograph No.: 3	Date: 3/26/2026	Direction Taken: North
<p>Description: View of exterior of Buildings #3 and #7; a 2-foot brick knee wall will remain and be used as the base of the new slope.</p>		

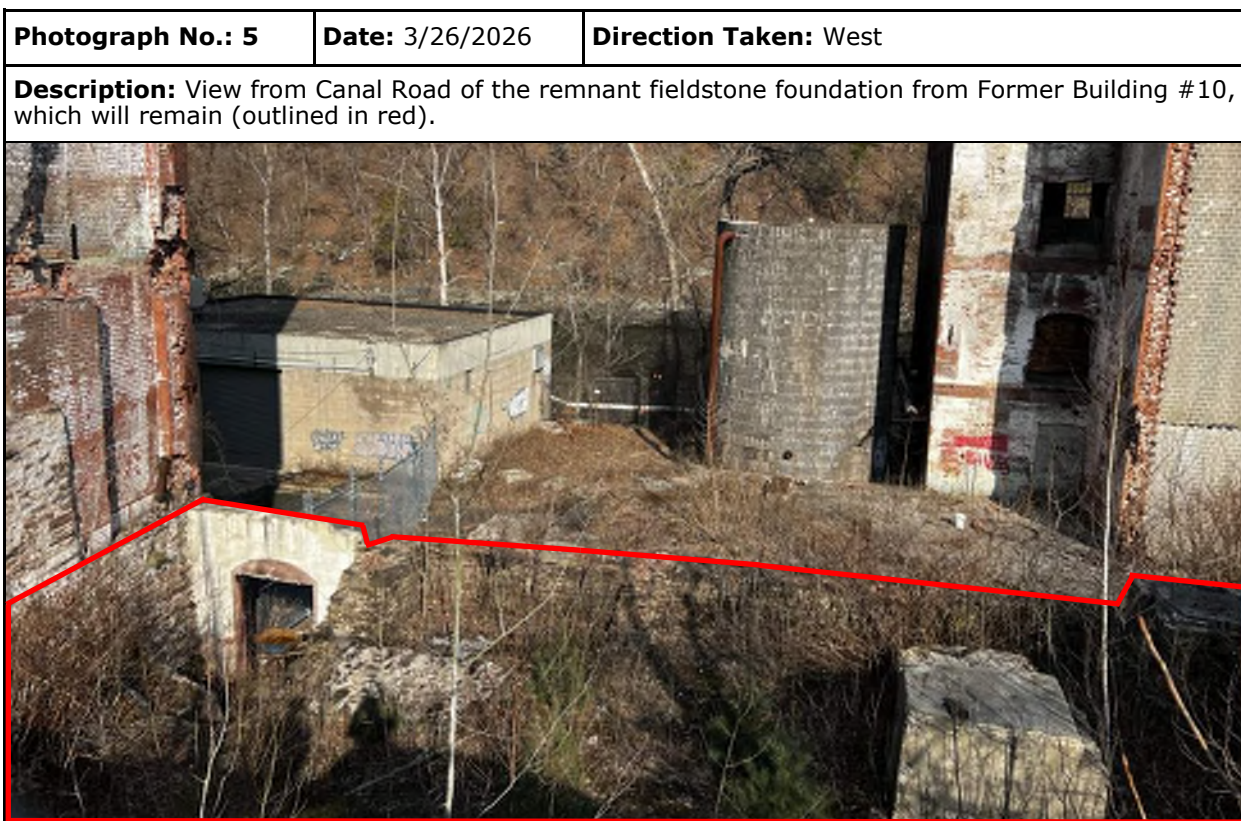
Photograph No.: 4	Date: 1/23/2026	Direction Taken: South
<p>Description: View of Building #11 fieldstone foundation; the fieldstone foundation walls (outlined in red) for Buildings #4, #5, #6, #7, #8 and #11 along canal road will remain.</p>		

Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA



**Attachment C:
Photographic Log**



Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

Photograph No.: 1	Date: 09/21/2018	Direction Taken: West
<p>Description: Aerial view of the Strathmore Mill Complex. The smokestack was demolished in 2020 after this photo was taken.</p>		

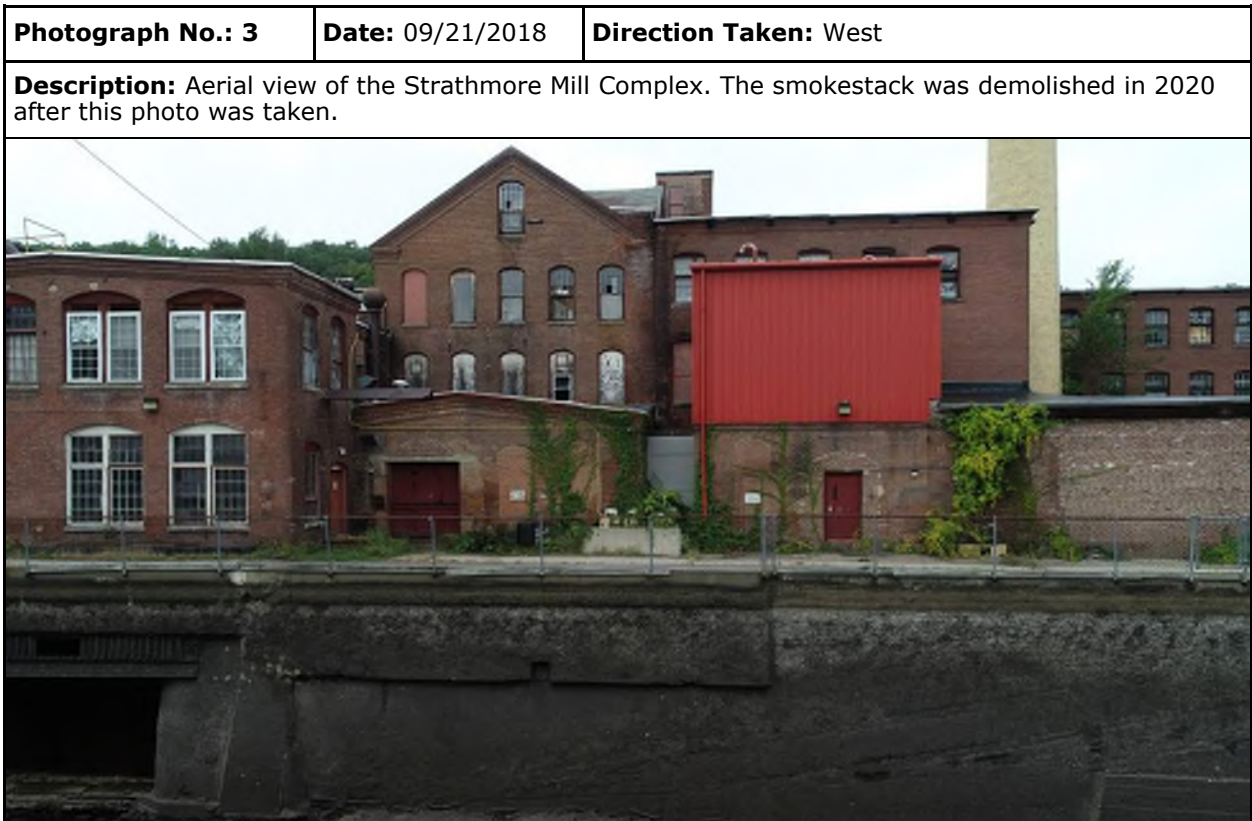
Photograph No.: 2	Date: 9/21/2018	Direction Taken: North
<p>Description: Aerial view of the Strathmore Mill Complex showing Buildings #2-#9 and the Connecticut River. The Smokestack was demolished in 2020 after this photo was taken.</p>		

Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

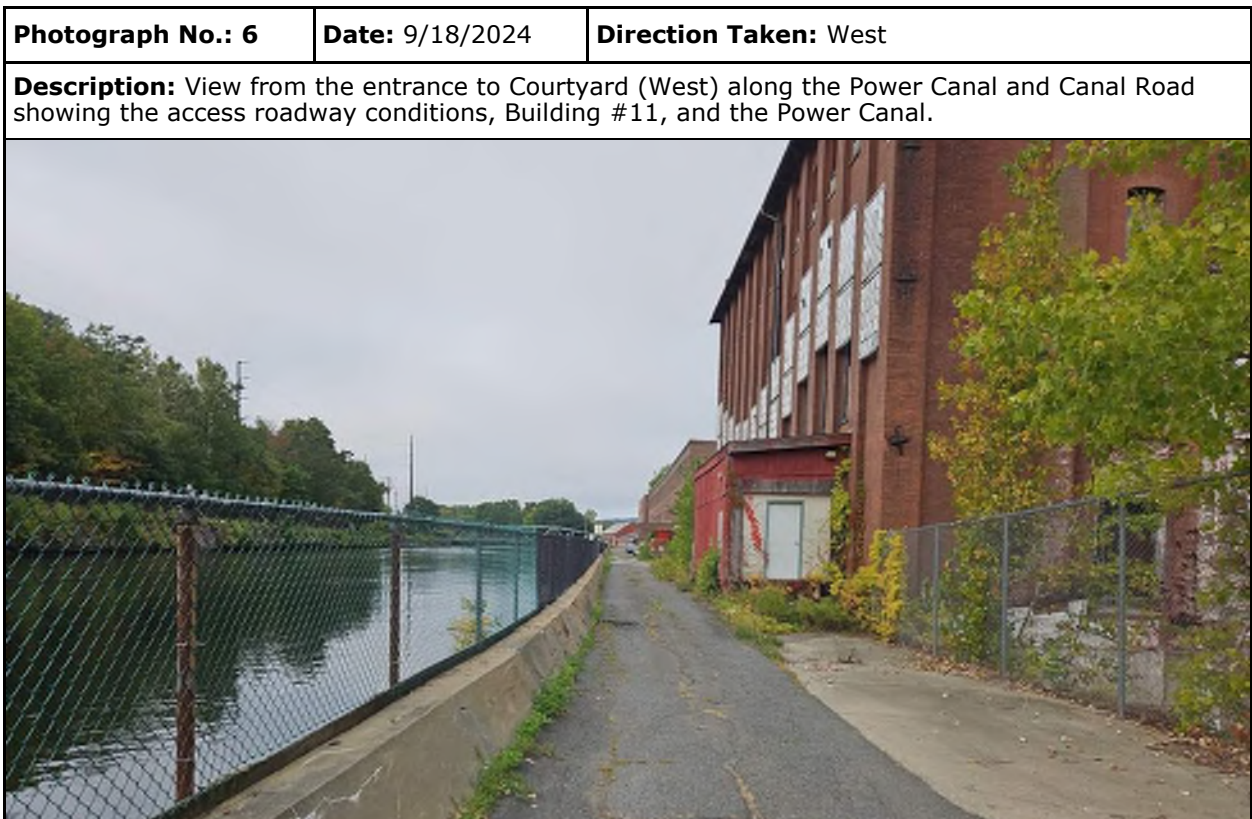
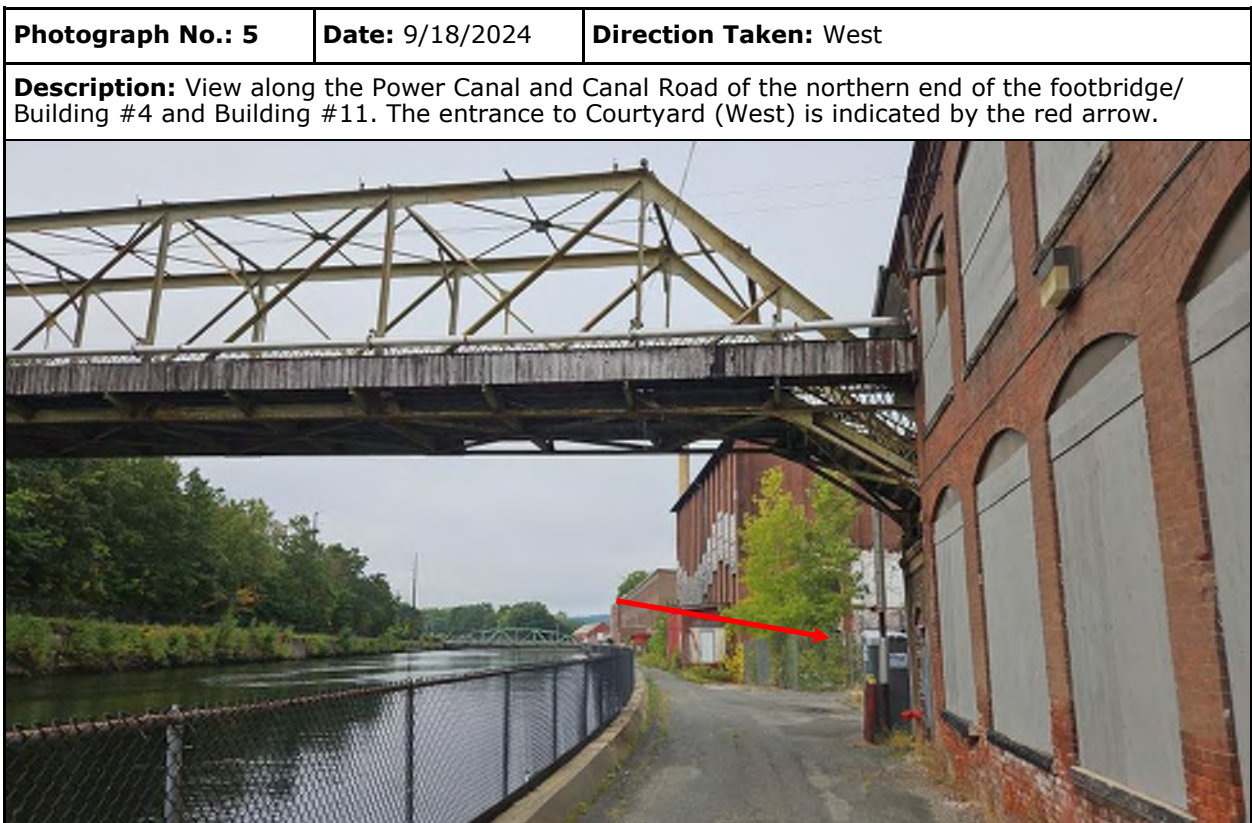


Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA



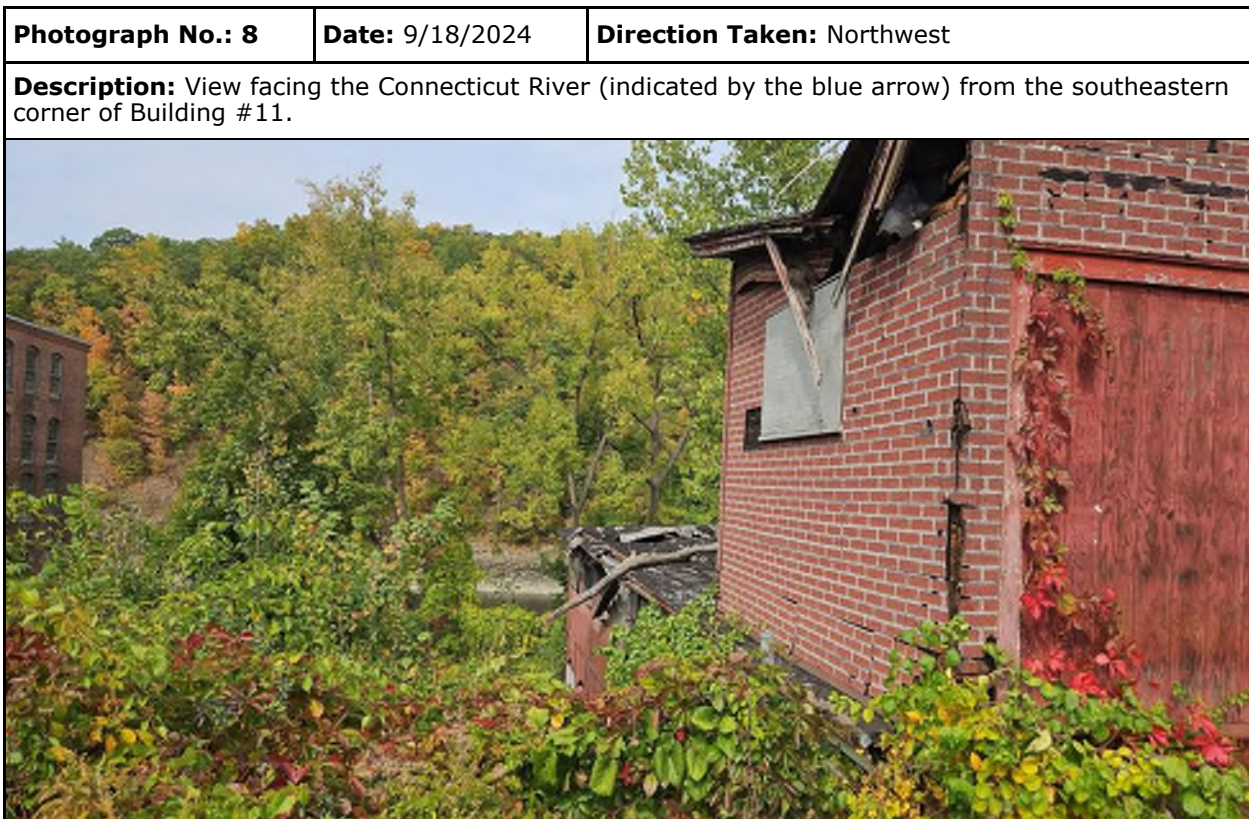
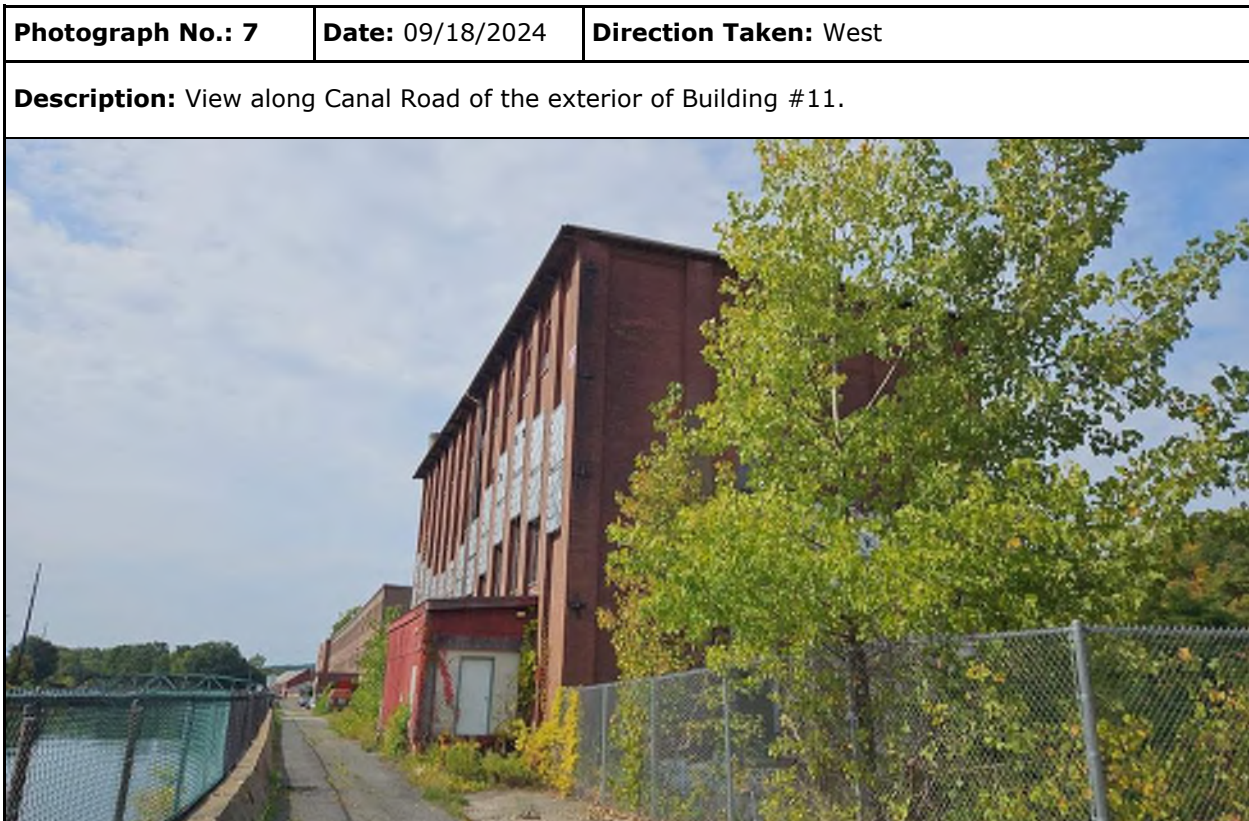
Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex

Location: Montague, MA



Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Strathmore Mill Complex

Client: Town of Montague

Location: Montague, MA

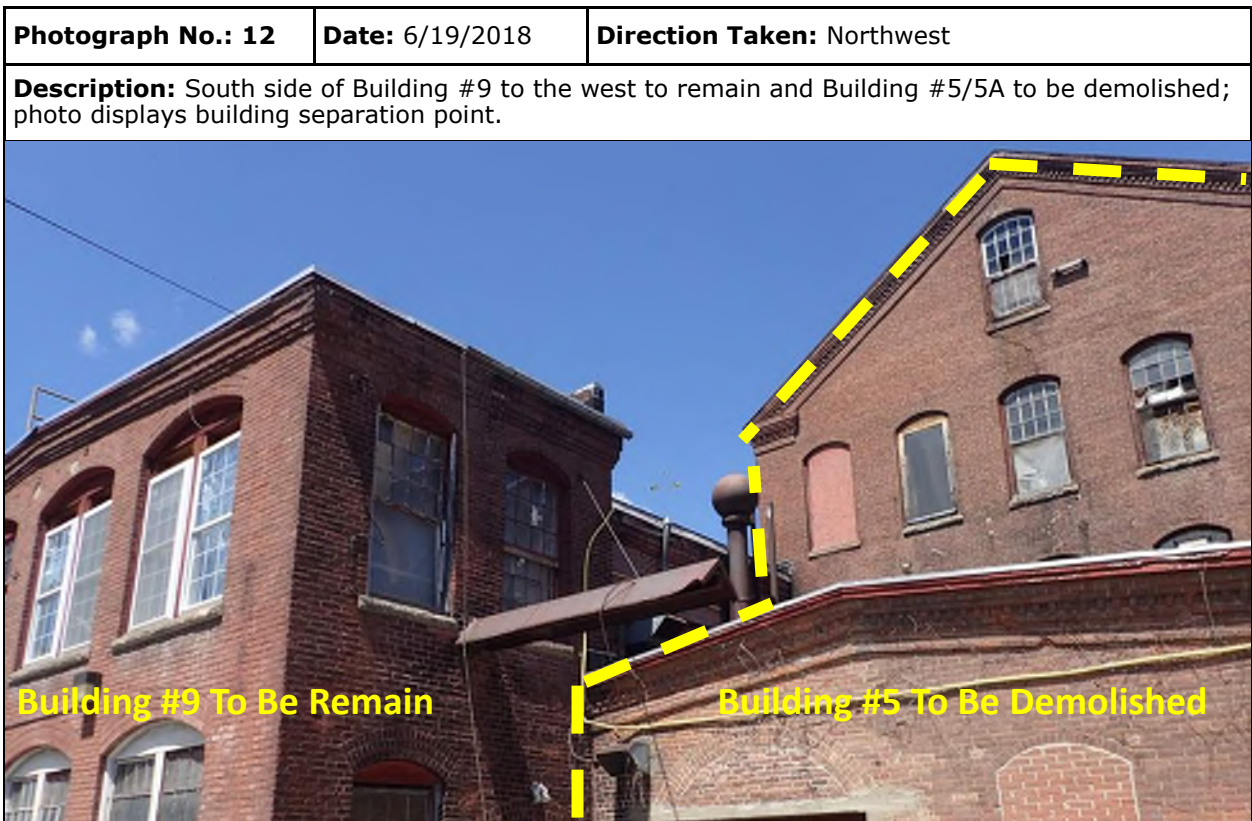
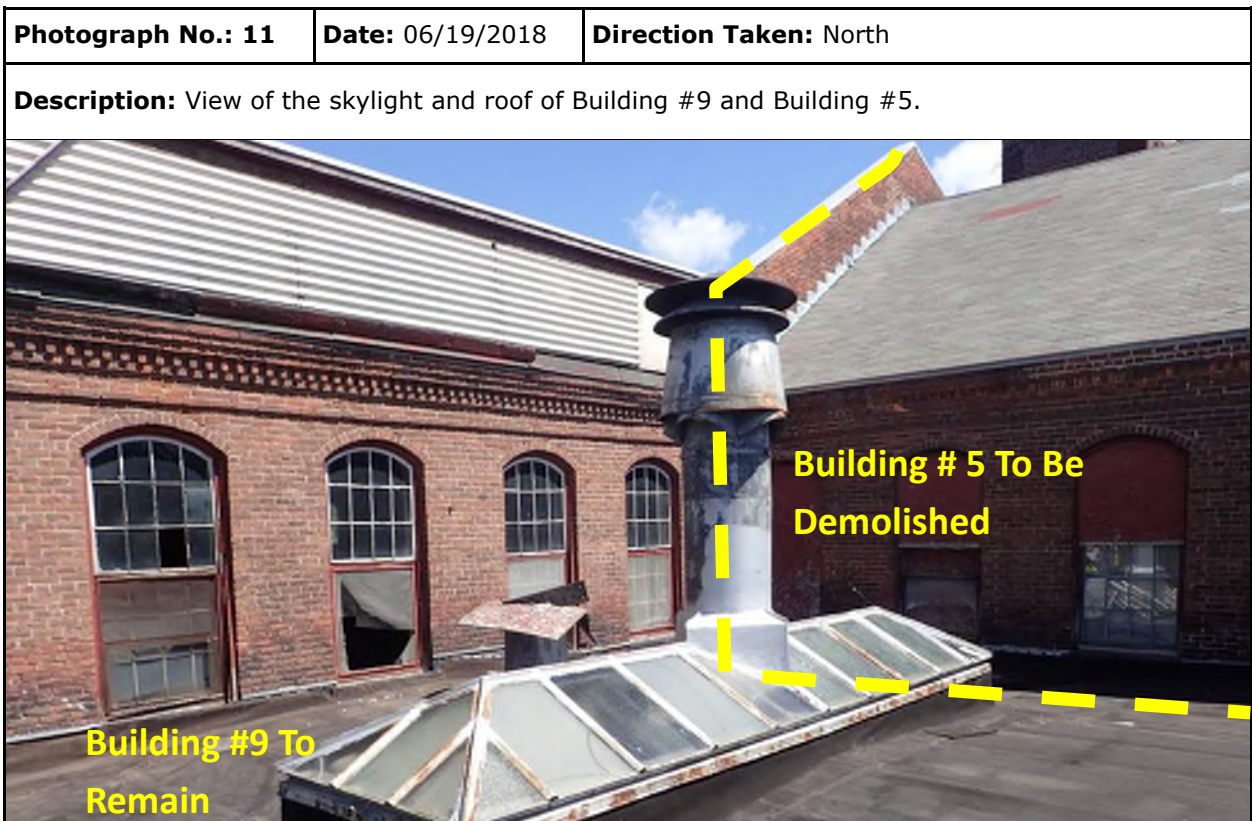


Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA



Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA




Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex

Location: Montague, MA

Photograph No.: 15	Date: 02/22/2019	Direction Taken: East
Description: View of existing conditions in the interior of Building #1, lowest level.		
		

Photograph No.: 16	Date: 09/18/2024	Direction Taken: North
Description: View of boiler brick in Building #1, lowest level.		
		

Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex

Location: Montague, MA

Photograph No.: 17	Date: 9/18/2024	Direction Taken: Northwest
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Description: View of the interior of Building #1.



Photograph No.: 18	Date: 9/18/2024	Direction Taken: North
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Description: View of the Building #1 foundation wall to remain.



Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

Photograph No.: 19	Date: 09/18/2024	Direction Taken: East
Description: View of the interior of Building #2.		

Photograph No.: 20	Date: 9/18/2024	Direction Taken: East
Description: View of the Connecticut River and Tailrace #2 from inside Building #2. Ground surface between the building foundation and river's edge is littered with brick and other building-related debris.		

Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

Photograph No.: 21	Date: 09/21/2019	Direction Taken: Southwest
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Description: View of the rear (north side) of Buildings #2 and #3 and the Connecticut River. Debris between the building foundations and the river will remain.



Photograph No.: 22	Date: 2/22/2019	Direction Taken: South
---------------------------	------------------------	-------------------------------

Description: View of the Tailrace #3 outlet, which is scheduled to be capped and deconstructed.



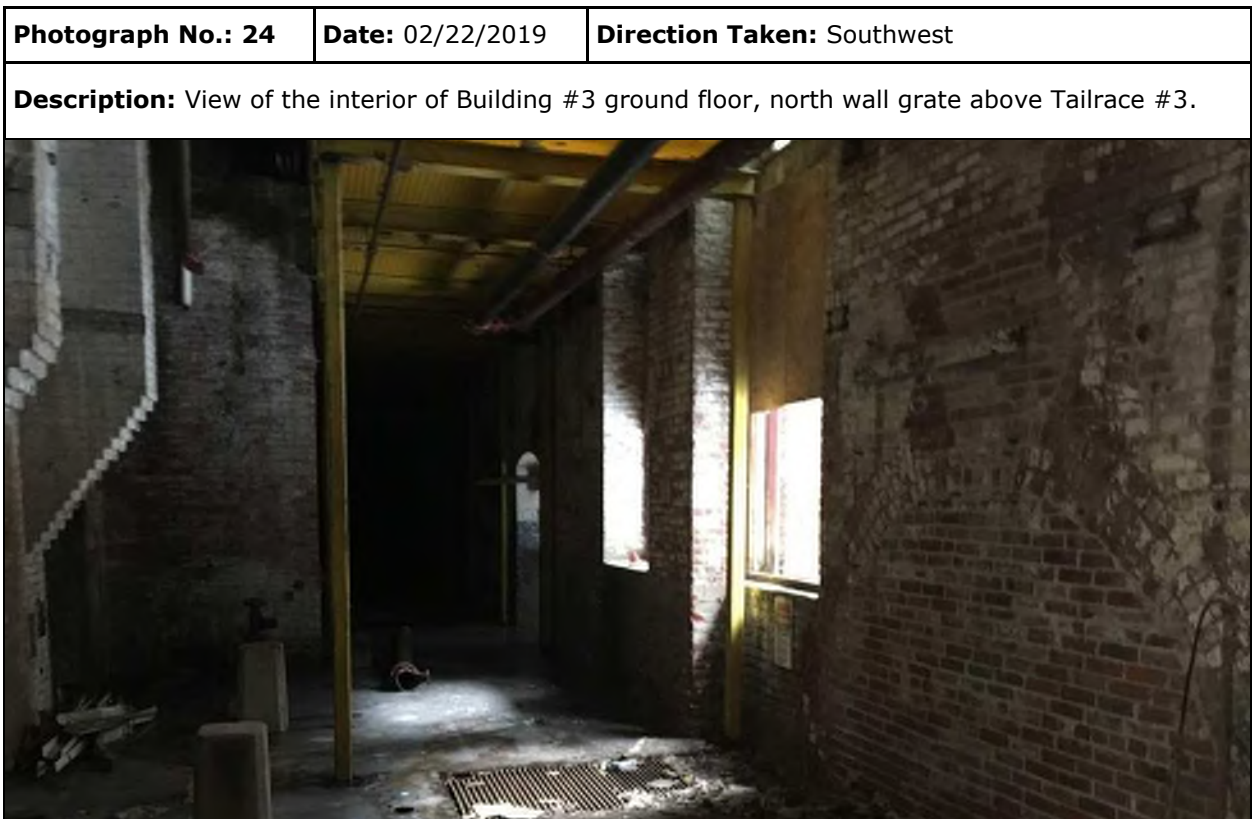
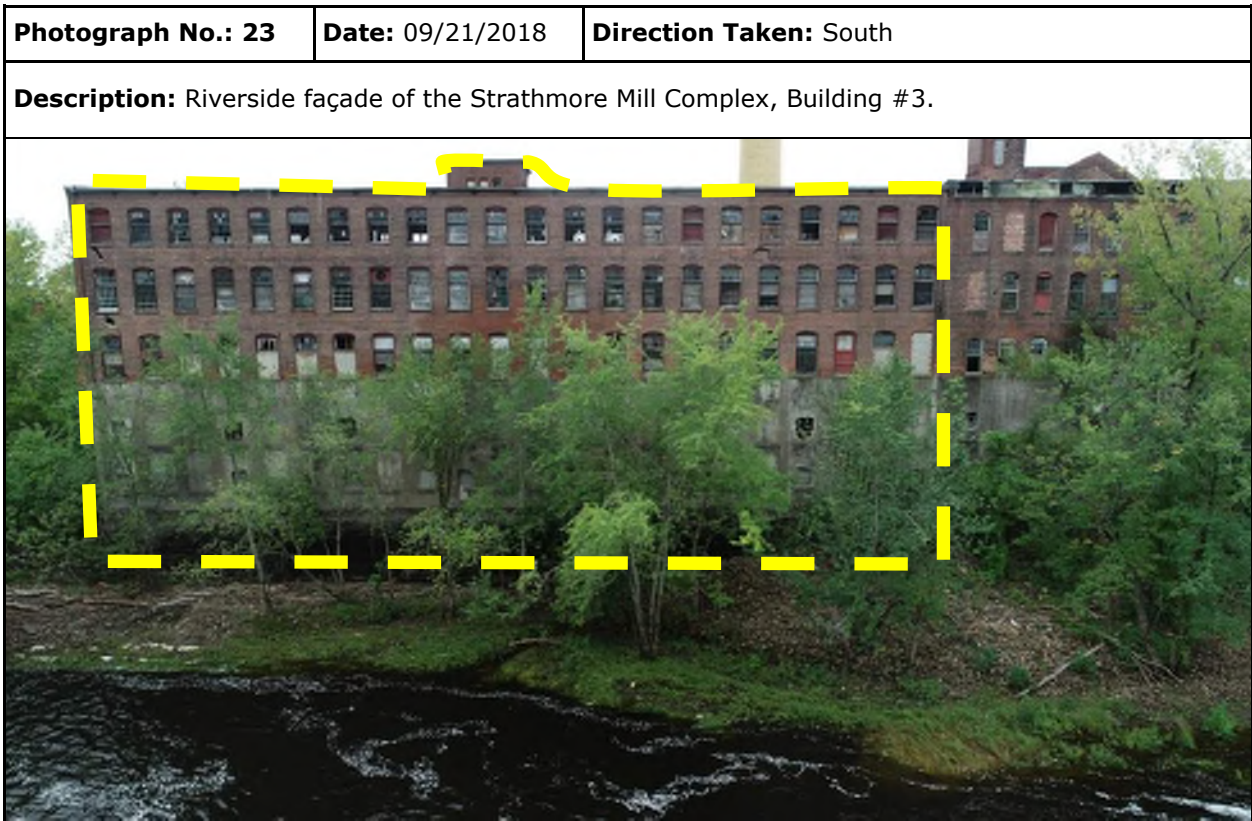
Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex

Location: Montague, MA



Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

Photograph No.: 25	Date: 02/02/2019	Direction Taken: West
Description: View of the interior of Building #3, 1 st floor.		

Photograph No.: 26	Date: 02/22/2019	Direction Taken: East
Description: View of the interior of Building #3, 2 nd floor.		

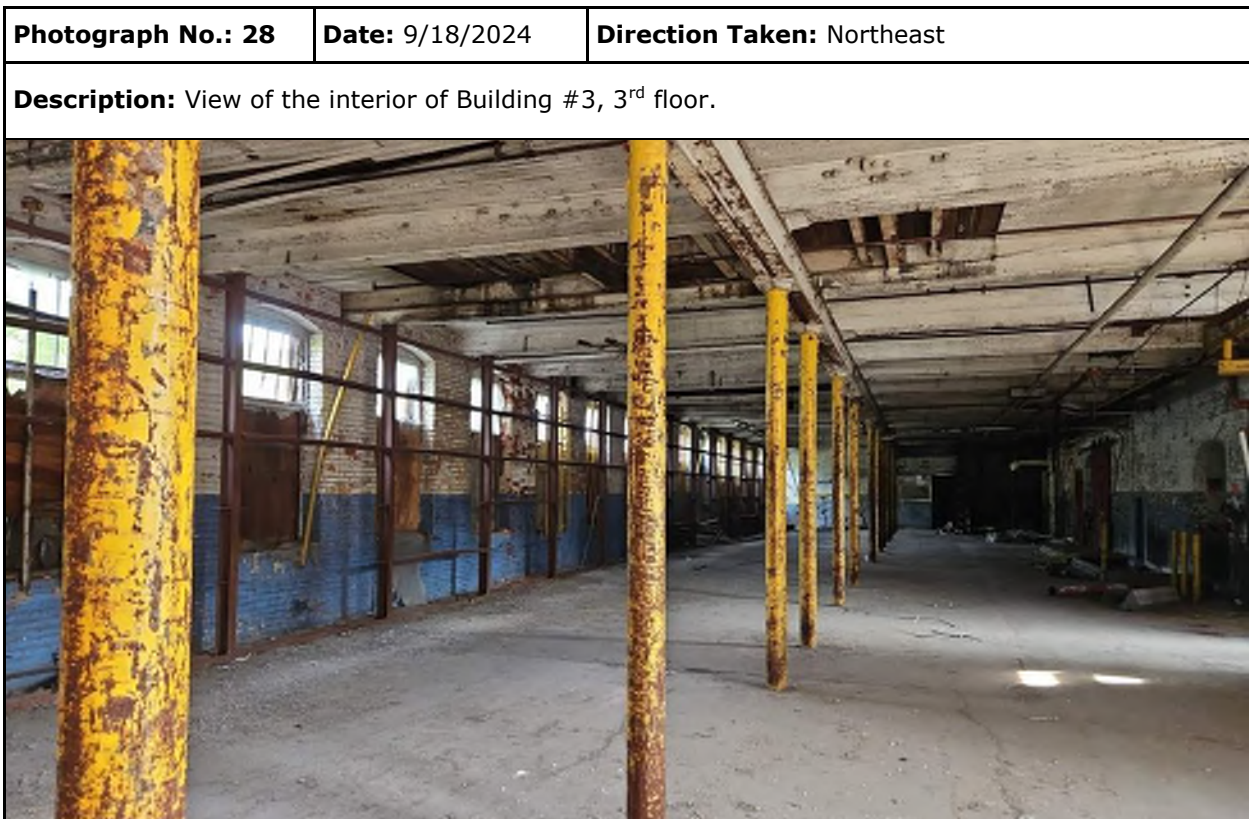
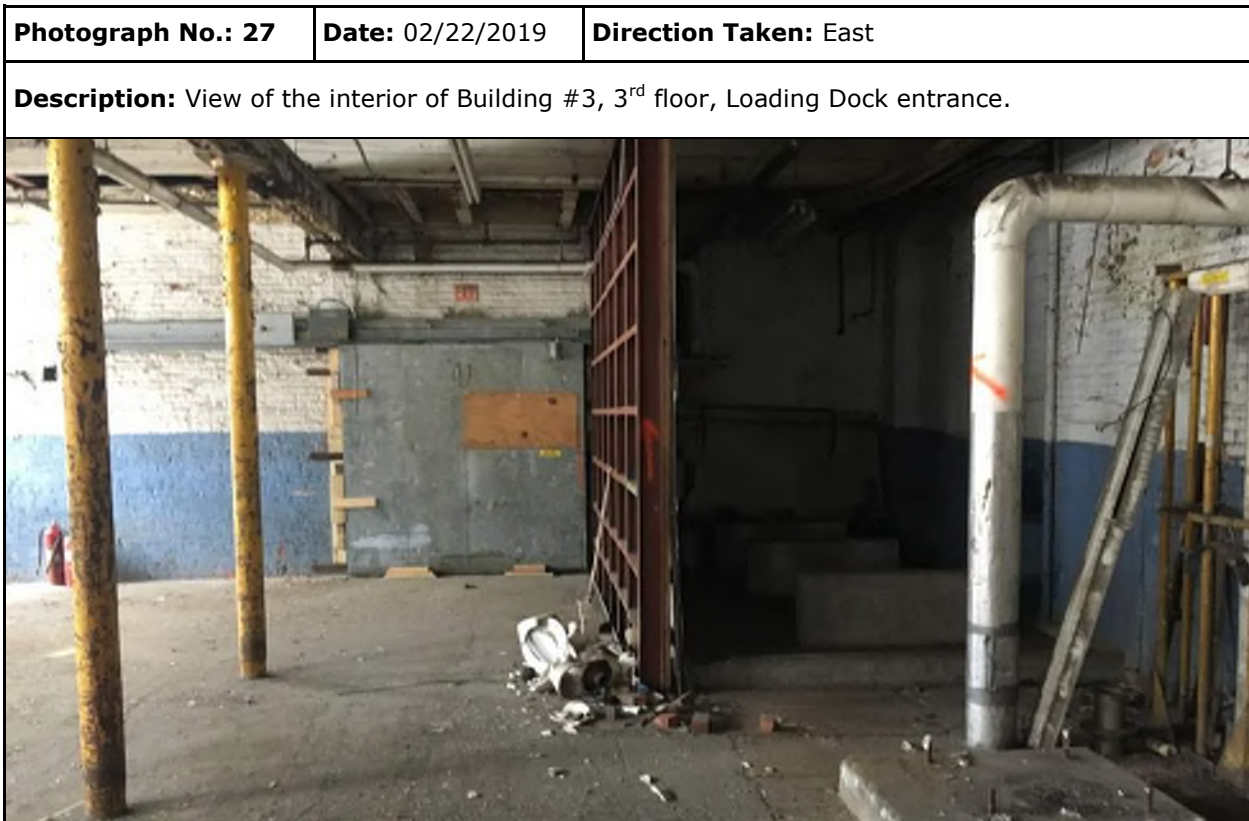
Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex

Location: Montague, MA

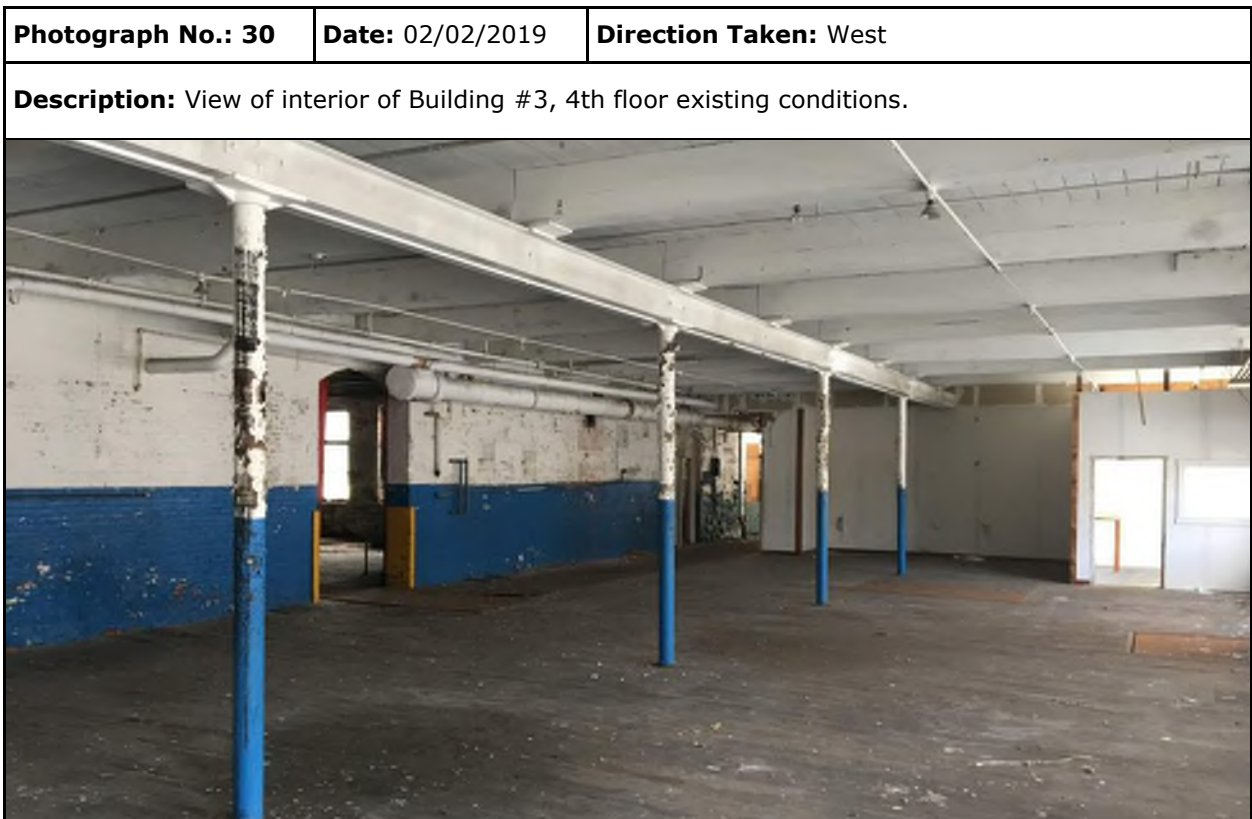
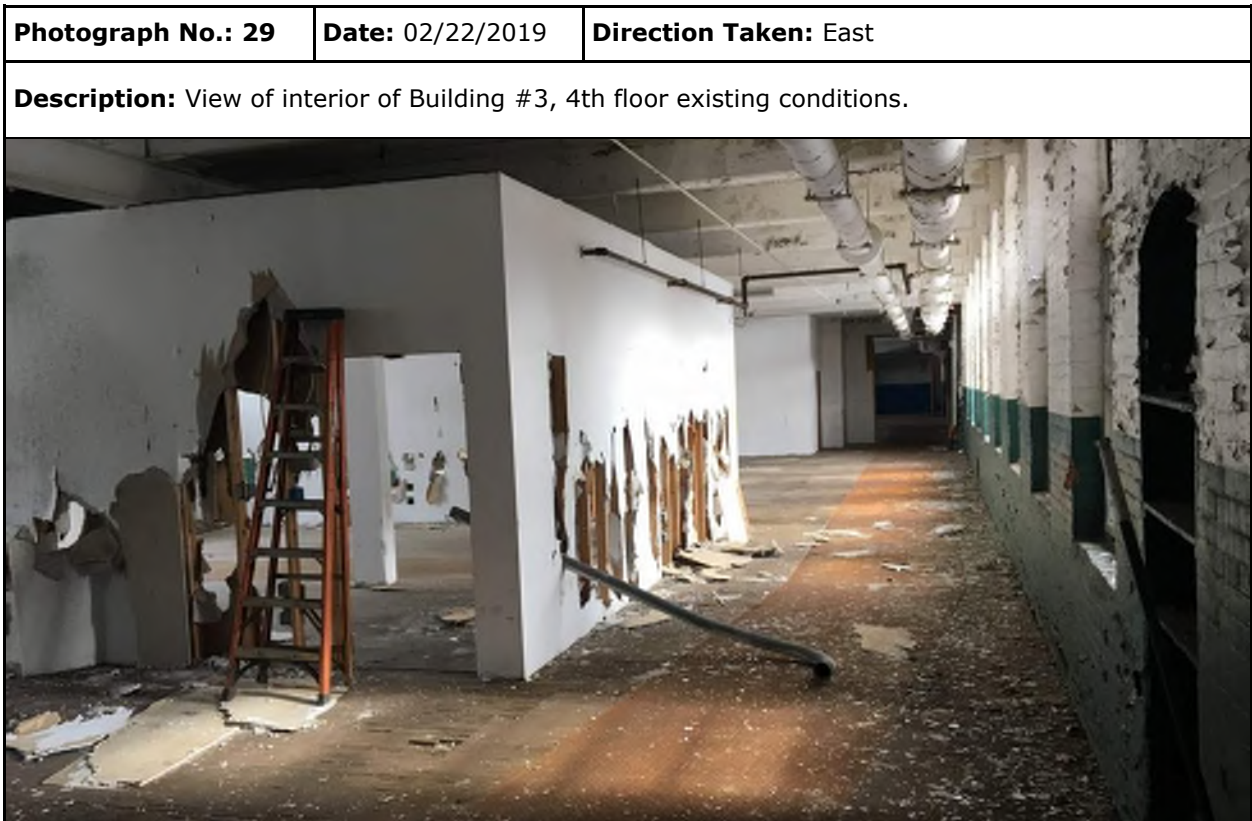


Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA



Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

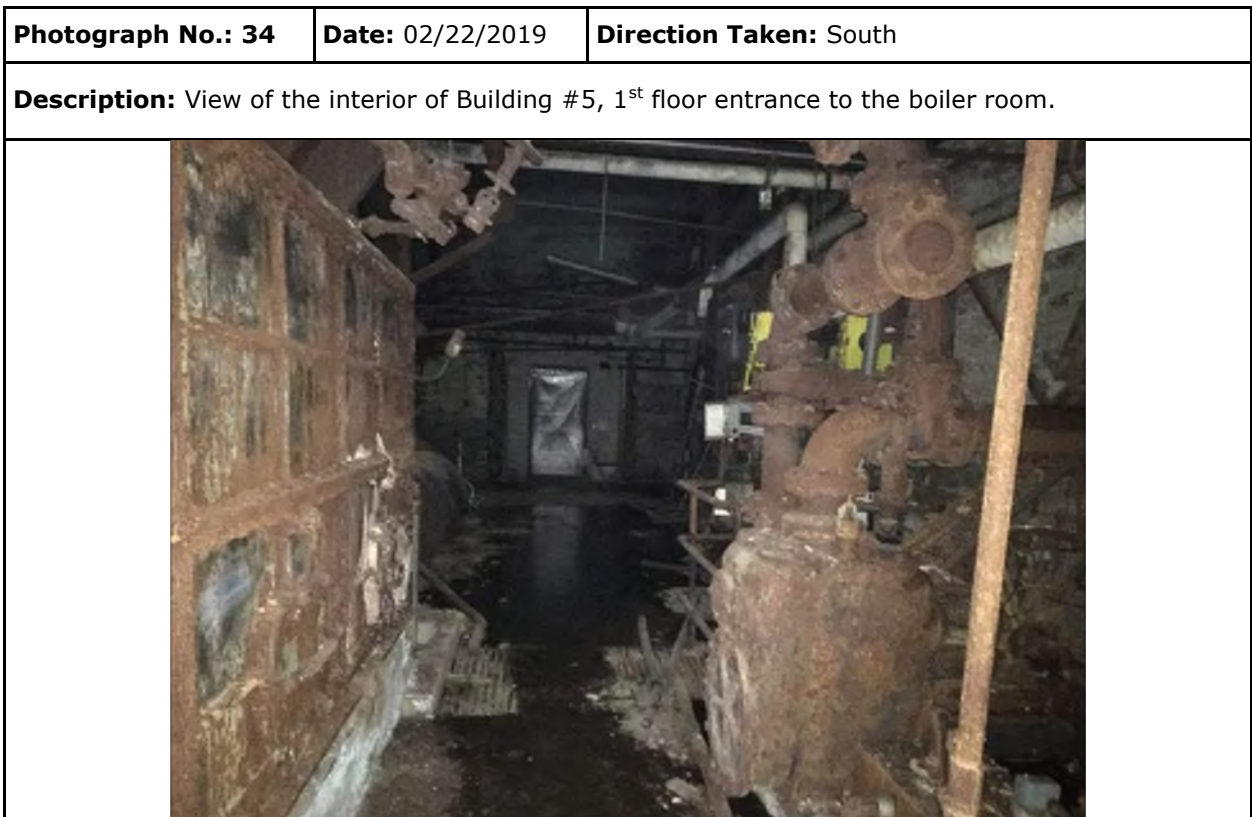
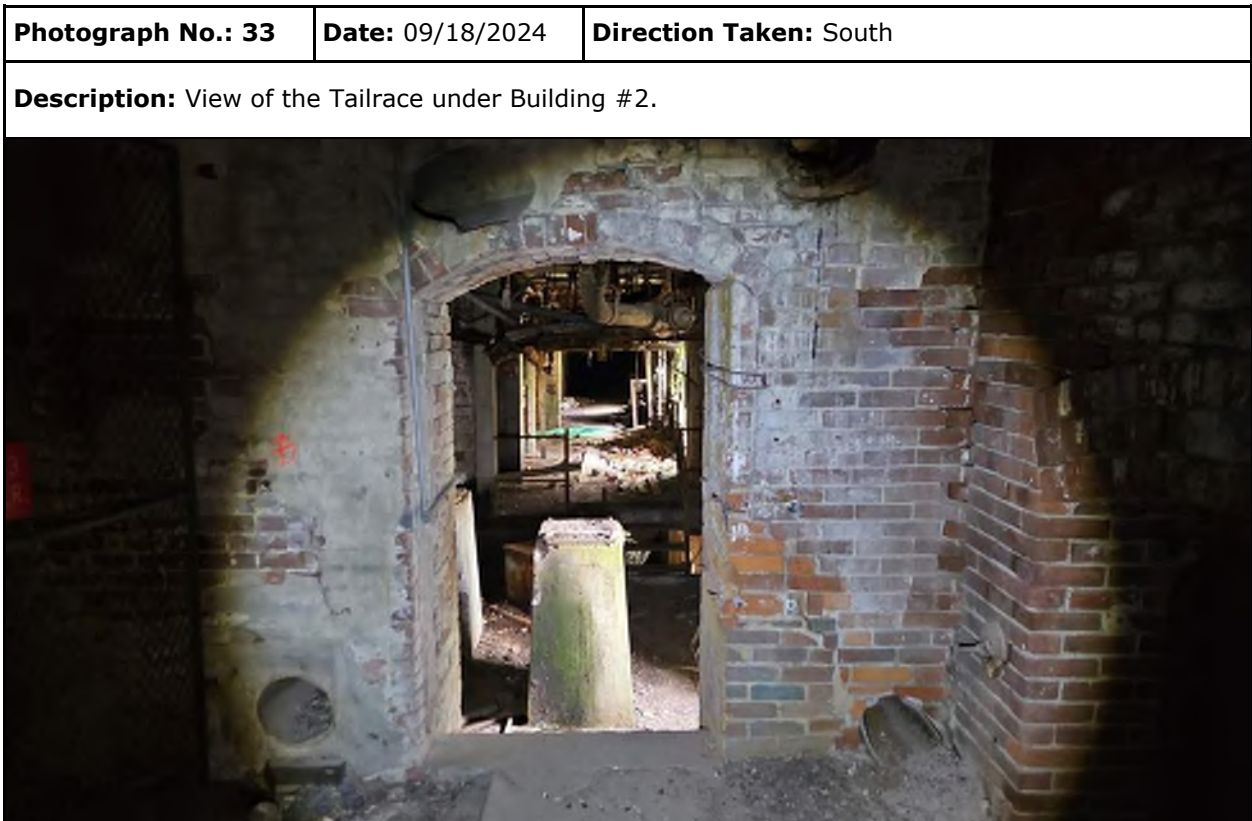


Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

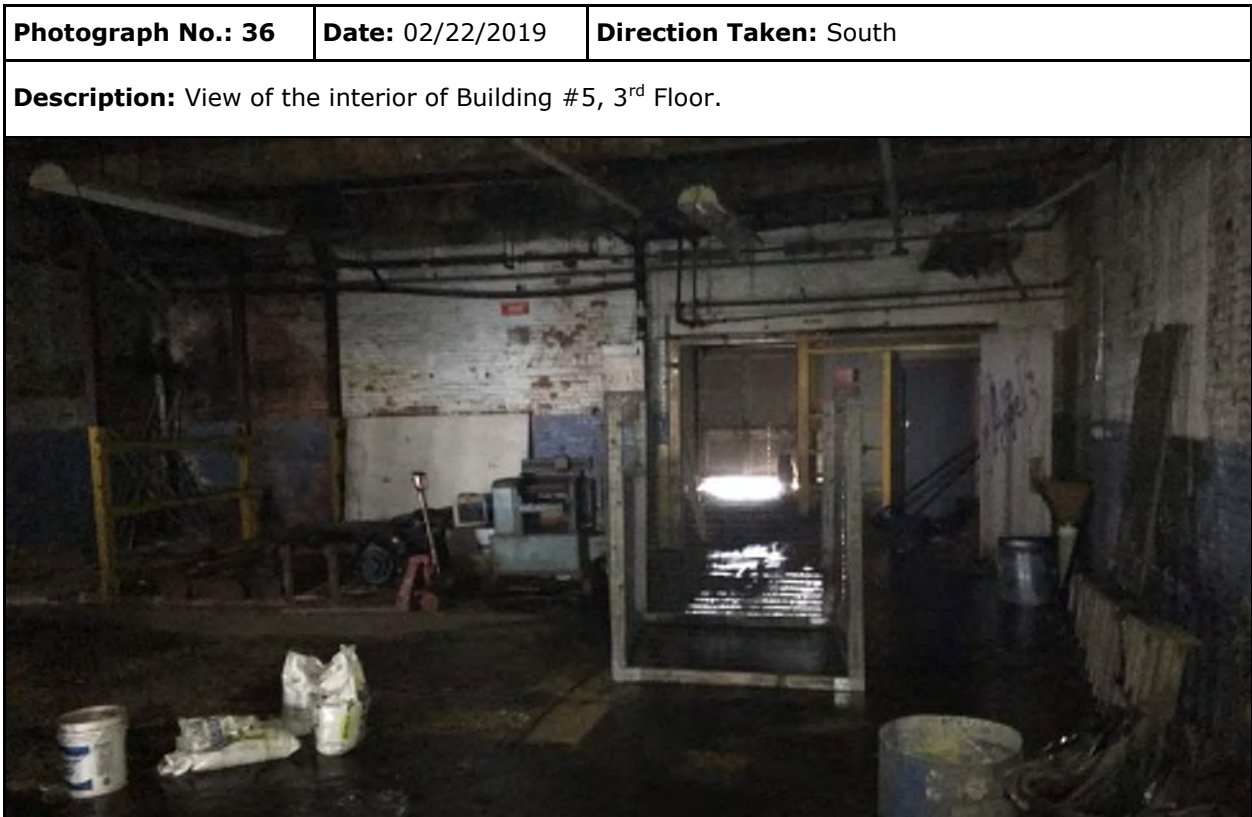
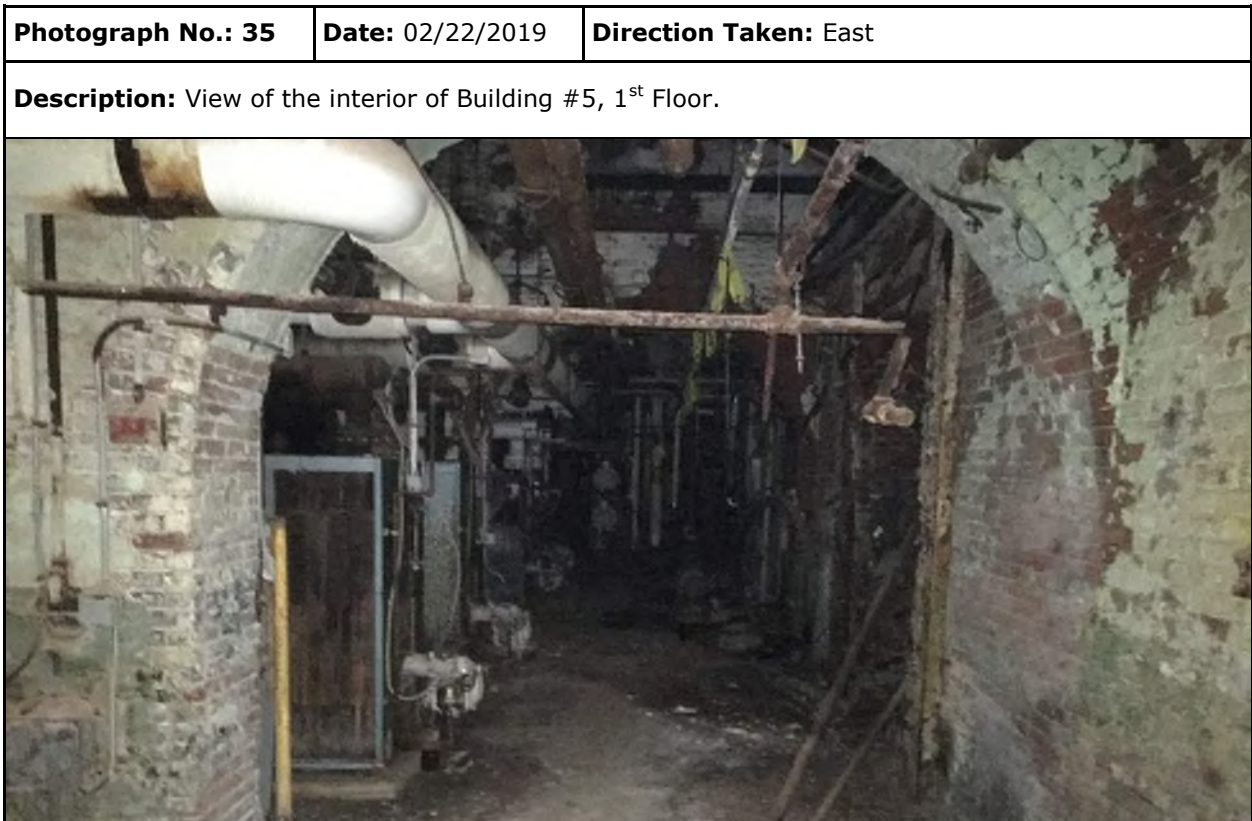


Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA



Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

Photograph No.: 37	Date: 02/22/2019	Direction Taken: South
Description: View of the interior of Building #5, 3 rd floor, south side entrance to Canal Road.		

Photograph No.: 38	Date: 02/22/2019	Direction Taken: South
Description: View of the interior of Building #5, 4 th floor.		

Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

Photograph No.: 39	Date: 02/22/2019	Direction Taken: South
<p>Description: View of the interior of Building #5, 4th floor. An approximately 25-foot by 25-foot portion of this area has collapsed since this photo was taken.</p>		

Photograph No.: 40	Date: 02/22/2019	Direction Taken: South
<p>Description: View of the interior of Building #5, 5th floor.</p>		

Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA



Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

Photograph No.: 43	Date: 09/18/2024	Direction Taken: North
Description: View from Canal Road of Buildings #5 and #6.		

Photograph No.: 44	Date: 9/18/2024	Direction Taken: East
Description: View of from Canal Road of Building #7.		

Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

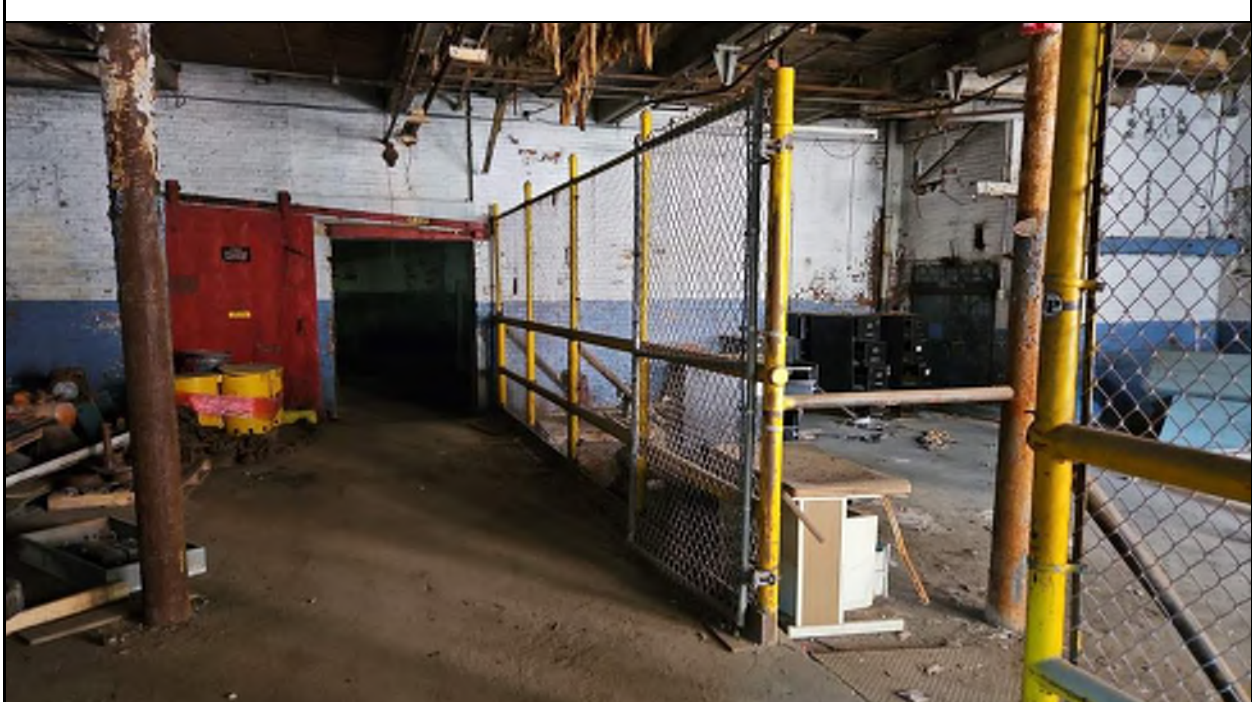
Strathmore Mill Complex

Location: Montague, MA

Photograph No.: 45	Date: 09/18/2024	Direction Taken: North
Description: View of the interior of Building #6, facing Courtyard (East) and the Smokestack.		



Photograph No.: 46	Date: 9/18/2024	Direction Taken: East
Description: Interior view of Building #6.		



Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

Photograph No.: 47	Date: 02/22/2019	Direction Taken: South
Description: View of the interior of Building #6, 1 st floor. View of the rear of boilers to be abated and demolished.		

Photograph No.: 48	Date: 02/22/2019	Direction Taken: North
Description: View of the interior of Building #6, 3 rd floor.		

Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex

Location: Montague, MA

Photograph No.: 49	Date: 02/22/2019	Direction Taken: South
Description: View of the interior of Building #6, 4 th floor.		
		

Photograph No.: 50	Date: 02/22/2019	Direction Taken: South
Description: View of the interior of Building #6, 5 th floor.		
		

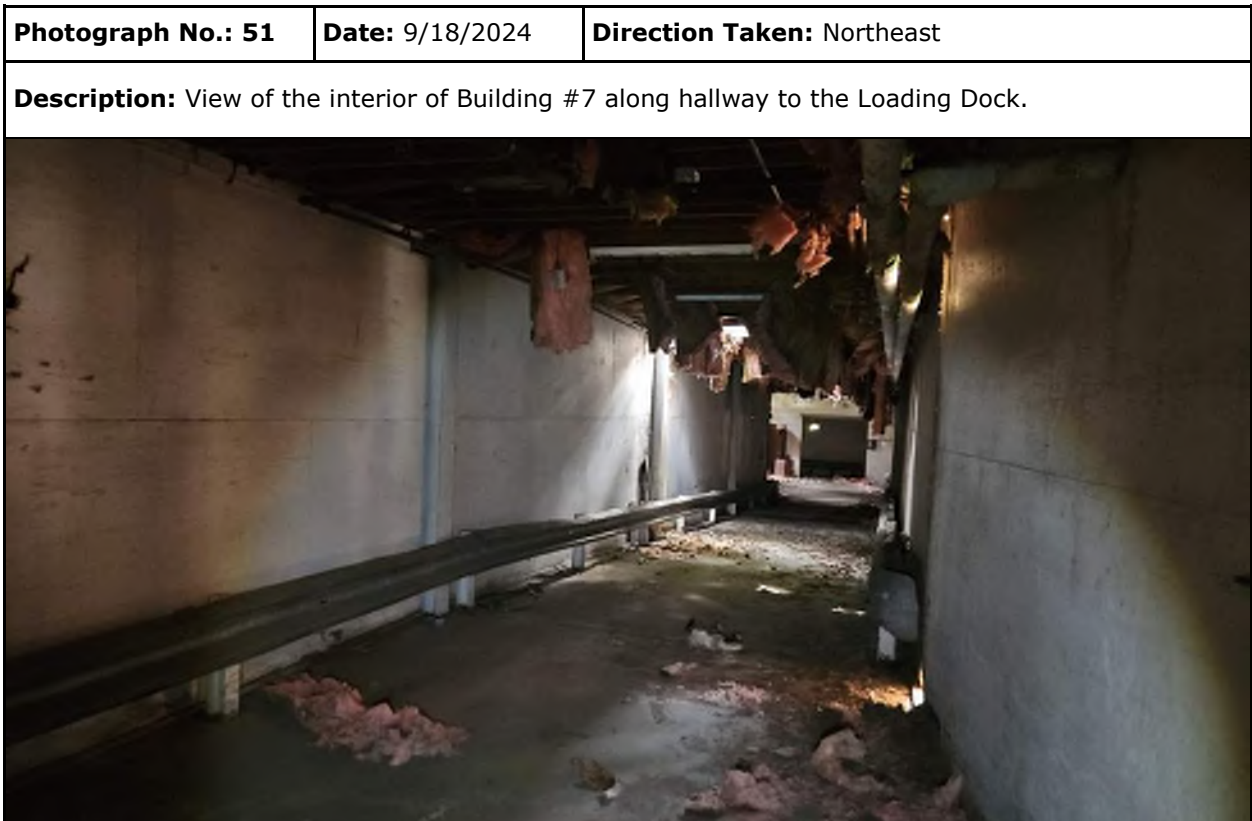
Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex

Location: Montague, MA



Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

Photograph No.: 53	Date: 02/22/2019	Direction Taken: South
Description: View of the interior of Building #7, 1 st floor, with varying interior levels.		

Photograph No.: 54	Date: 02/22/2019	Direction Taken: West
Description: View of the interior of Building #7, 1 st floor.		

Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

Photograph No.: 55	Date: 02/22/2019	Direction Taken: North
Description: View of the interior of Building #7, 2 nd floor.		

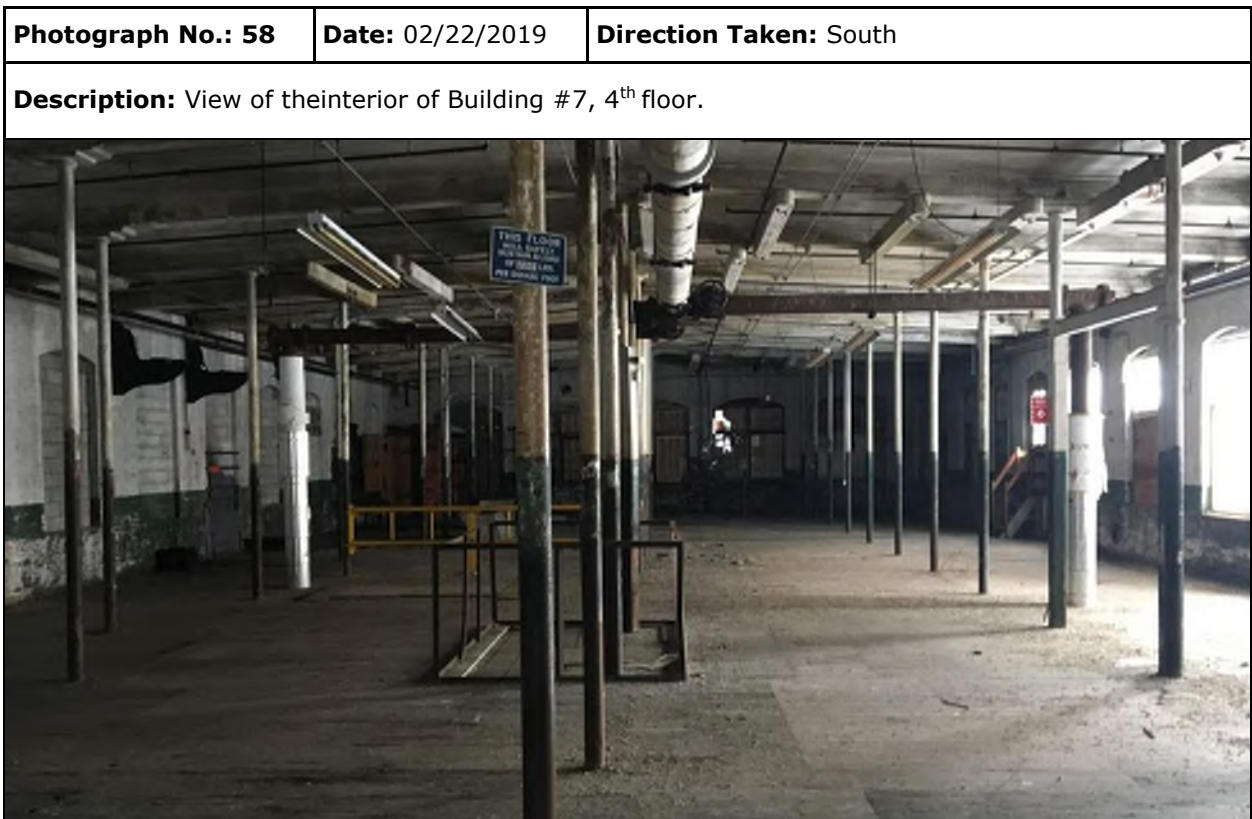
Photograph No.: 56	Date: 02/22/2019	Direction Taken: South
Description: View of the interior of Building #7, 2 nd floor.		

Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA



Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

Photograph No.: 59	Date: 02/22/2019	Direction Taken: West
Description: View of the interior of Building #8, 2 nd floor. There is no access to the 1 st floor of this building.		

Photograph No.: 60	Date: 02/22/2019	Direction Taken: West
Description: View of the interior of Building #8, 2 nd floor.		

Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex

Location: Montague, MA

Photograph No.: 61	Date: 02/22/2019	Direction Taken: West
Description: View of the interior of Building #8, 3 rd floor.		

Photograph No.: 62	Date: 02/22/2019	Direction Taken: West
Description: View of the interior of Building #8, 3 rd floor.		

Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

Photograph No.: 63	Date: 02/22/2019	Direction Taken: North
Description: Loading Dock; view of building supports and elevation change between Building #7 and adjacent area of overgrown vegetation.		

Photograph No.: 64	Date: 02/22/2019	Direction Taken: West
Description: Area of overgrown vegetation located around/and under the Loading Dock and concrete pipe support structures to be demolished. The pipes were previously removed.		

Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

Photograph No.: 65	Date: 02/22/2019	Direction Taken: East
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Description: Closer view of the concrete pipe structures to be demolished located around and under the Loading Dock. The pipes were previously removed.



Photograph No.: 66	Date: 02/02/2019	Direction Taken: South
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Description: Interior view of the Loading Dock.



Project: Strathmore Mill Demolition Project

Job Number: M-5003-012

Client: Town of Montague

Strathmore Mill Complex
Location: Montague, MA

Photograph No.: 67	Date: 02/22/2019	Direction Taken: East
Description: View of Building #7 from the power canal.		

Photograph No.: 68	Date: 09/18/2024	Direction Taken: Northeast
Description: View of the proposed Staging Area east of the Loading Dock attached to Building #3.		

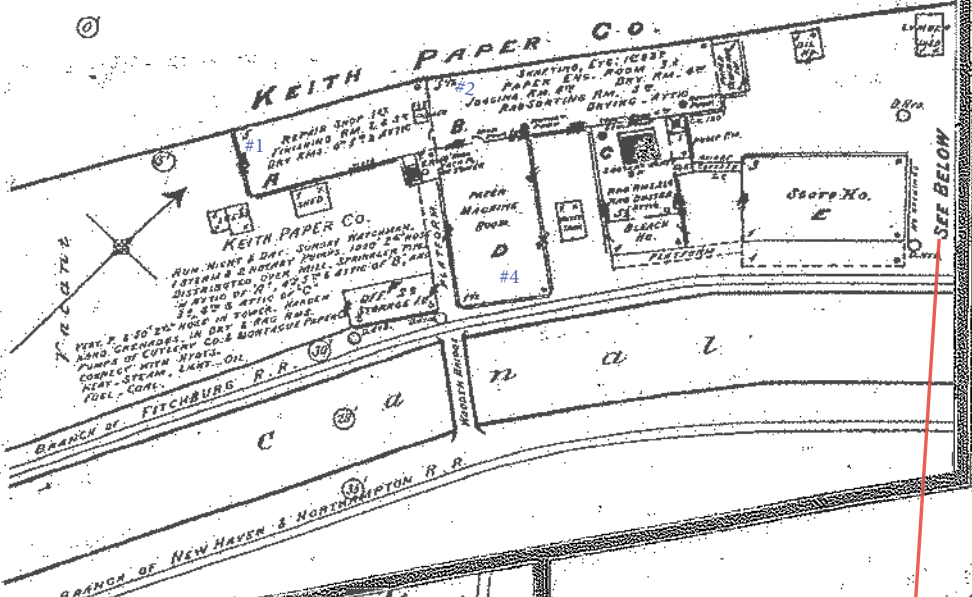
**Attachment D:
Sanborne Fire Insurance Maps**



Connecticut River

NOTICE.—The SANBORN MAP & PUBLISHING CO. (limited) will loan, to parties desiring an copy of this map, a Draftsman, with the mutual understanding and agreement that it is to be used exclusively by their General or Local Agent, and, after the further agreement that, if not so used, one of the two copies will be returned to said Company.

JUNE 1884
TURNERS FALLS
 MASS.

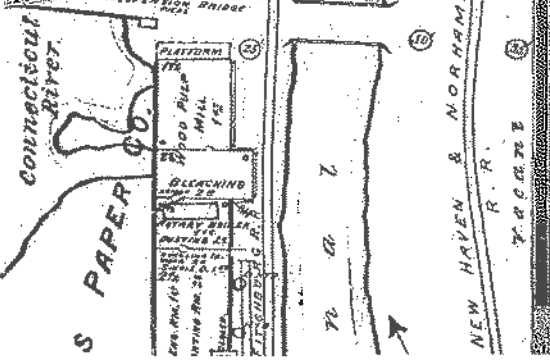
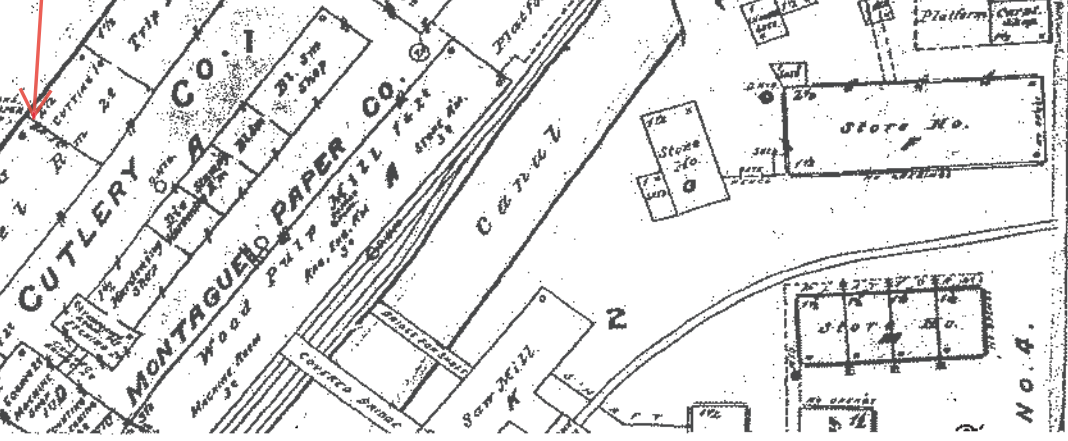


Connecticut River

MONTAGUE PAPER CO.

JOHN RUSSELL CUTLERY CO.

2 NIGHTWATCHMEN AND ELECTRIC CLOCK.
 GOOD SUPPLY OF FIRE DETROITED OVER WORKS
 CORRY PUMP IN 'A' USE IN COMMON WITH DUNN
 CO. WATER PIPES CONNECTED WITH PUMP UP
 KEITH & MONTAGUE PAPER CO. STEAMER
 TINES AND GRATES & PAILS IN ATTIC OF 21
 PAILS IN NEARLY ALL ROOMS
 HEAT PIPES & HOSE EVERY FLOOR & CORNERS
 V. P. WITH HOSE EACH FLOOR
 LIGHT BRICK CORNICES ON ALL BUILDING
 POWER, WATER,
 HEAT, STEAM,
 LIGHT, GAS (MADE ON PREMISES)
 FUEL, COAL.



No. 4.

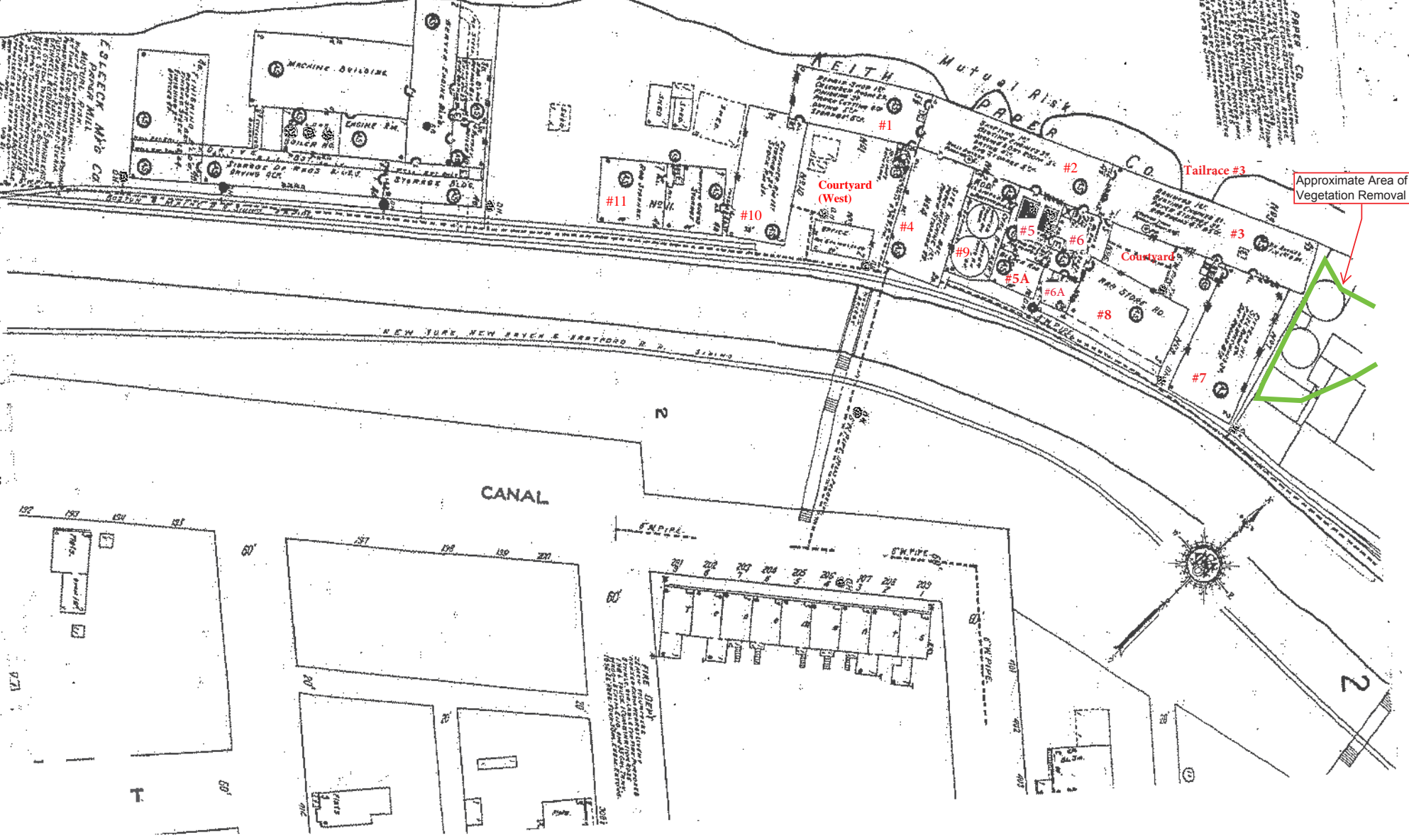
No. 4.

Prior to 1914

Connecticut River

Keith Paper Co. Construction
Detailed description of the construction work and materials used in the building of the Keith Paper Co. mill, including references to various reports and specifications.

SELECT M.B. CO.
MILL
Detailed description of the mill's operations and equipment, including references to various reports and specifications.



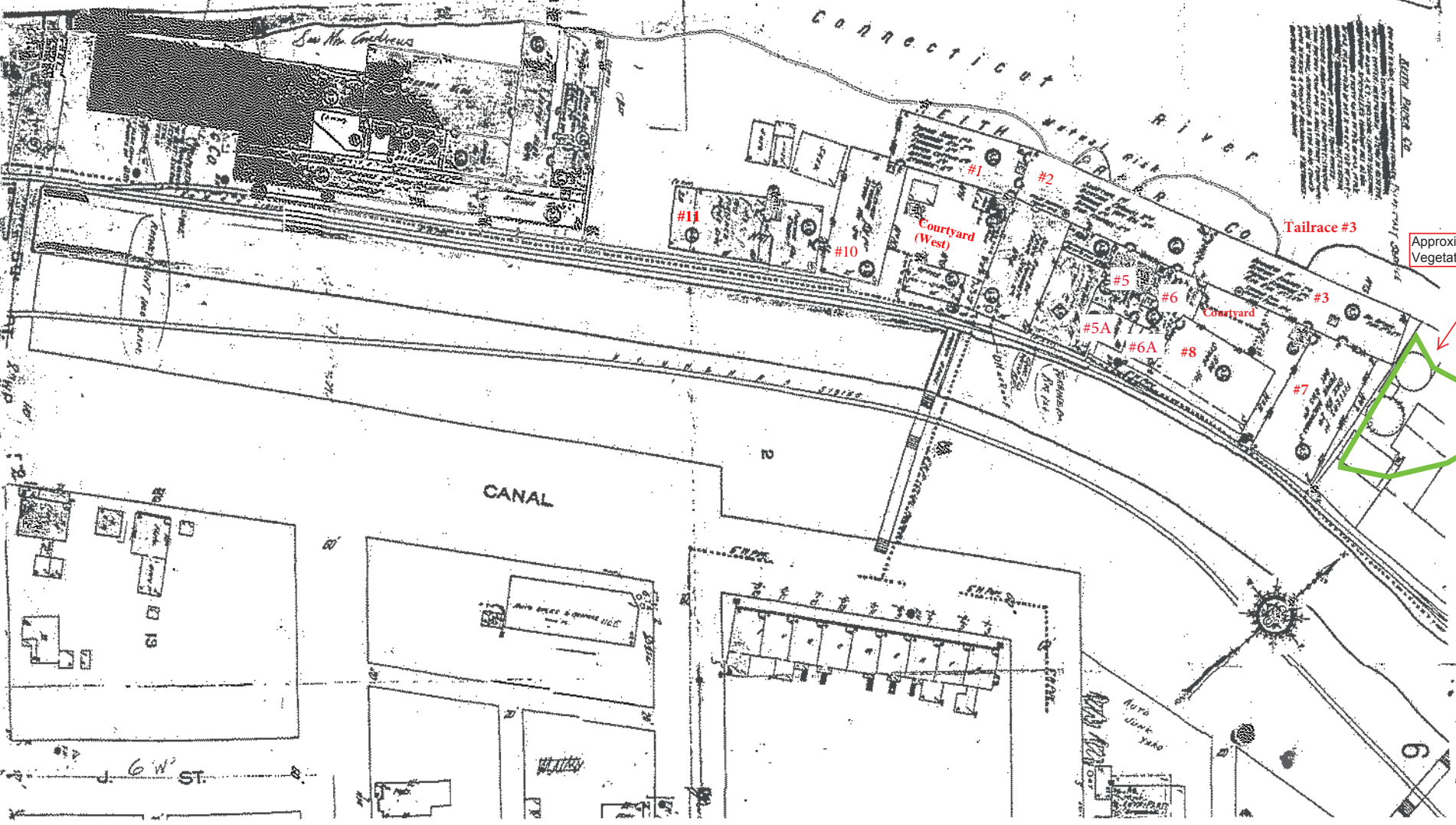
Approximate Area of Vegetation Removal



1889

Essex MS

1940



Approximate Area of Vegetation Removal

WMS

**Attachment E:
MHC Correspondence**





May 24, 2019

The Commonwealth of Massachusetts

William Francis Galvin, Secretary of the Commonwealth
Massachusetts Historical Commission

Steven Ellis
Town Administrator
Town of Montague
1 Avenue A
Montague, MA 01376

RE: Strathmore Mill Complex Demolition, 8 and 20 Canal Road, Montague, MA; MHC# RC.66308

Dear Mr. Ellis:

Staff of the Massachusetts Historical Commission (MHC) have reviewed the Project Notification Form (PNF) that was submitted by Tighe & Bond, received at this office on April 26, 2019, for the project referenced above. The staff of the MHC have the following comments.

The proposed project consists of the demolition of Strathmore Mill Complex buildings #3, #5/5A, #6/6A, #7, #8, the smokestack, and loading dock at building #3. The information provided indicates that the project will require a beneficial use determination from the Massachusetts Department of Environmental Protection and a Massachusetts Endangered Species Act (MESA) Regulatory review by the Massachusetts Division of Fisheries and Wildlife.

Review of the MHC's *Inventory of Historic and Archaeological Assets of the Commonwealth* indicates that the Strathmore Mill Complex, historically known as the Keith Paper Mill, on Canal Street (MNT.131) is listed in the State and National Registers of Historic Places as a contributing element of the Turner's Falls Historic District (MNT.H).

Demolition of these buildings constitutes an "adverse effect" (950 CMR 71.05(a) and 950 CMR 71.07 (2)(b)(3)) through the destruction or alteration of all or part of a State Register property. Pursuant to 950 CMR 71.07(3), the MHC looks forward to consulting with the Town of Montague to explore alternatives that would eliminate, minimize, or mitigate the adverse effect of the proposed demolition. The MHC hereby initiates the MHC's consultation process pursuant to 950 CMR 71.07(3).

The MHC requests that a copy of the Project Notification Form and information be forwarded to the Montague Historical Commission. Copies of any comments received from the Montague Historical Commission should be submitted to MHC.

These comments are offered to assist in compliance with M.G.L. Chapter 9, sections 26-27C (950 CMR 71.00) and MEPA (301 CMR 11). Please do not hesitate to contact Elizabeth Sherva of my staff if you have any questions.

Sincerely,

A handwritten signature in cursive script that reads "Brona Simon".

Brona Simon
State Historic Preservation Officer
Executive Director
Massachusetts Historical Commission

cc: Natural Heritage & Endangered Species Program, Massachusetts Division of Fisheries and Wildlife
Dan Hall, Massachusetts Department of Environmental Protection – Western Region
Ed Gregory, Montague Historical Commission
Tracy Adamski, Tighe & Bond

220 Morrissey Boulevard, Boston, Massachusetts 02125
(617) 727-8470 • Fax: (617) 727-5128
www.sec.state.ma.us/mhc



The Commonwealth of Massachusetts
William Francis Galvin, Secretary of the Commonwealth
Massachusetts Historical Commission

June 3, 2020

Jeff Singleton
Montague Historical Commission
Montague Town Hall
1 Avenue A
Turners Falls, MA 01376

RE: Strathmore Mill Complex Demolition, 8 and 20 Canal Road, Montague, MA; MHC# RC.66308

Dear Mr. Singleton:

Your recent email message regarding the proposed demolition of the smokestack at the Strathmore Mill was referred to my attention for response. In your message, you express your concerns that the Massachusetts Historical Commission "verbally approved the demolition" of the smokestack "without consulting with the local historical commission."

There is a misunderstanding of the MHC staff's recent telephone conversation with an engineer from Tighe & Bond and how that conversation was conveyed or interpreted by the Town.

In the conversation, it was disclosed to the MHC staff that the Town was proposing to remove the smokestack using Town funding. Because no state or federal funding or permits were required, the MHC staff replied that MHC had no review authority over the removal of the smokestack and that it would not require the filing of an MHC Project Notification Form. Please note that the MHC's review is triggered whenever state or federal funding, license or permits are required for a project. The MHC has no power or authority to review projects that are solely funded or permitted by the Town. If the Town will use any state or federal funding for the removal of the smokestack, review by the MHC is required.

The MHC stands by its May 24, 2019 letter. Whenever the Town or its consulting engineer responds to MHC's letter, the MHC will consult with the Montague Historical Commission under 950 CMR 71.07(3) (see enclosed copy of MHC regulations).

Sincerely,

A handwritten signature in cursive script that reads "Brona Simon".

Brona Simon
State Historic Preservation Officer
Executive Director
Massachusetts Historical Commission

Enclosure (950 CMR 71)

xc: Walter Ramsey, Montague Town Planner
Ed Gregory, Chair, Montague Historical Commission
Tracy Adamski, Tighe & Bond

